

# JUDICIAL DRIVE TRAIL CONNECTION

CITY OF FAIRFAX, VIRGINIA

CITY CONTRACT # 14009-2014-TIM

## NOTICE REQUIRED

CONTRACTORS SHALL NOTIFY OPERATORS WHO MAINTAIN UNDERGROUND UTILITY LINES IN THE AREA OF PROPOSED EXCAVATION AND OR BLASTING AT LEAST TWO (2) WORKING DAYS, BUT NOT MORE THAN TEN (10) WORKING DAYS, PRIOR TO COMMENCEMENT OF EXCAVATION OR DEMOLITION. NAMES AND TELEPHONE NUMBERS OF THE OPERATORS OF UNDERGROUND UTILITY LINES APPEAR BELOW. THESE NUMBERS SHALL ALSO BE USED TO SERVE IN AN EMERGENCY CONDITION.

Fairfax County Dept of Utilities

Dominion Power

Verizon Communications

Washington Gas

Mason LD

MISS UTILITY

1-800-552-7001

## EMERGENCY

Police/Fire/Rescue: 911

Non-Emergency No.: (703) 993-8370



SCALE: 1" = 50'

## OWNER: CITY OF FAIRFAX, VIRGINIA

10455 ARMSTRONG ST  
FAIRFAX VA 22031  
CATHY SALGADO

## ENGINEER: TIMMONS GROUP

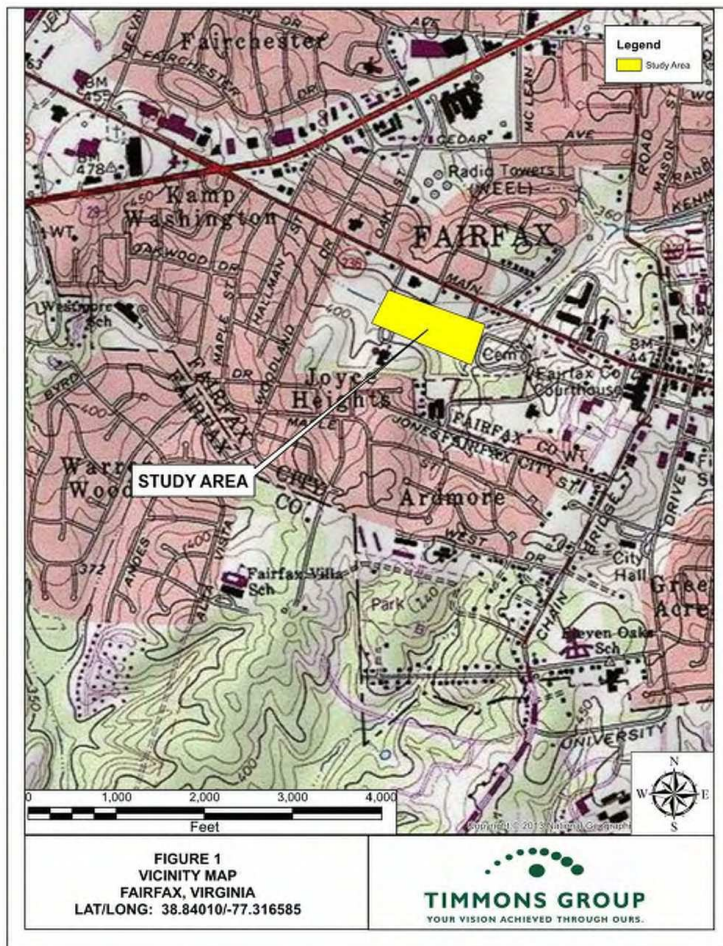
20110 ASHBROOK PLACE, SUITE 100  
ASHBURN, VIRGINIA 20147  
CASEY KIGHT, LA  
PHONE: (703) 554-6710

## PROJECT NARRATIVE

THE JUDICIAL DRIVE TRAIL IS A PUBLIC PROJECT LOCATED IN AND COORDINATED BY THE CITY OF FAIRFAX. IT IS PROPOSED TO BE CONSTRUCTED ALONG THE SOUTH SIDE OF ACCORNS CREEK AND RUN EASTWEST BETWEEN JUDICIAL DRIVE AND PRESBYTERIAN WAY. THE CITY INTENDS TO CONNECT THE PROPOSED JUDICIAL DRIVE TRAIL TO EXISTING PORTIONS OF THE TRAIL THAT ALREADY EXIST. THE TRAIL IS PROPOSED TO BE DESIGNED IN ACCORDANCE WITH THE VDOT SHARED USE PATH DESIGN STANDARDS AND SPECIFICATIONS.

THE PROPOSED GEORGE SWINDEY TRAIL WILL BE DESIGNED IN ACCORDANCE WITH THE FOLLOWING VDOT SHARED USE PATH DETAILS AND SPECIFICATIONS:

- VERTICAL CLEARANCE: 8' MINIMUM TO THE BOTTOM OF ANY SIGN OR OVERHANGING VEGETATION
- DESIGN SPEED: 15 MPH MINIMUM
- HORIZONTAL ALIGNMENT: 7% CROSS SLOPE & SUPERELEVATION SLOPE MAXIMUM (AASHTO, RECOMMEND)
- HORIZONTAL CURVE: 60' RADIUS MINIMUM
- GRADE: 5% SLOPE MAXIMUM
- SIGHT DISTANCE: VARIES BASED ON GRADE, DESIGN SPEED AND ASCENDING/DESCENDING TRENDS (TABLES A-4-B & A-4-C)
- STOPPING DISTANCE: VARIES BASED ON GRADE, DESIGN SPEED AND ASCENDING/DESCENDING TRENDS (TABLES A-4-B & A-4-C)
- SHARED USE PATH WIDTH: 13 MINIMUM
- SHOULDER WIDTH: 2' MINIMUM (BOTH SIDES)
- SHOULDER GRADE: 6% MAXIMUM
- LATERAL CLEARANCE: 8' MINIMUM FROM SHARED USE PATH EDGE
- LATERAL WATER HAZARD: 8' MINIMUM FROM SHARED USE PATH EDGE
- STEEP GRADE CLEARANCE: 8' MINIMUM FROM SHARED USE PATH EDGE
- PHYSICAL BARRIERS IN ELEVATION: 10' MINIMUM
  - SLOPES 2:1 OR STEEPER, WITH A DROP OF 4" FEET OR GREATER
  - SLOPES 3:1 OR STEEPER, WITH A DROP OF 8" FEET OR GREATER
  - SLOPES 3:1 OR STEEPER, ADJACENT TO A PARALLEL WATER HAZARD OR OTHER OBVIOUS HAZARD
- SIGNAGE AND MARKING IN ACCORDANCE WITH VDOT DETAILS AND SPECIFICATIONS FOR SHARED USE PATHS
- PAVEMENT STRUCTURE (SHARED USE PATH DETAIL A-5-B-1):
  - TOP COAT: 7" ASPHALT CONCRETE (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5) (SMA 5)
  - BASE MATERIAL: 8" AGGREGATE BASE MATERIAL (TYPE 1) (OR 1" NUMBER 21A OR 2" #6)
  - SURFACE: REGULAR FILL MATERIAL OR NATIVE SOIL



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DATE: 09.22.2017  
DRAWN BY: CMK  
DESIGNED BY: CMK  
CHECKED BY: CMK  
SCALE: SHEET PLAN

**TIMMONS GROUP**

JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
COVER SHEET

JOB NO.: 35061.030  
SHEET NO.: C0.0

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CITY OF FAIRFAX NOTES:

- 1. A STREET OPENING PERMIT IS REQUIRED FOR ANY WORK IN A CITY RIGHT-OF-WAY OR EASEMENT...
2. ALL SIDEWALKS, CURBS, GUTTERS, DRIVEWAYS, STREETS, STORM PIPES, WATER LINES, SANITARY SEWER, ENDWALLS AND RFP-RAPS MUST BE INSPECTED BY THE CITY...
3. INSPECTIONS PERFORMED BY THE FACILITIES INSPECTOR WILL REQUIRE A FOUR-HOUR NOTICE PRIOR TO INSPECTIONS.

GENERAL STANDARDS

- 1. THE PUBLIC WORKS DIRECTOR MUST BE NOTIFIED ONE WEEK PRIOR TO PRE-CONSTRUCTION CONFERENCE...
2. A PRE-CONSTRUCTION MEETING WILL BE REQUIRED THREE DAYS PRIOR TO ANY CONSTRUCTION...
3. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS FOR PARKING CONSTRUCTION EQUIPMENT...
4. ALL CONSTRUCTION SHALL CONFORM TO THE LATEST CITY OF FAIRFAX STANDARDS...
5. TRAFFIC SIGNS FOUND TO BE IN THE WAY AT CONSTRUCTION SITES SHALL BE REMOVED OR RELOCATED ONLY BY PERSONNEL IN THE SIGN & SIGNAL CREW...
6. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARDS SET FORTH IN THE LATEST VERSION OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK...
7. PRIVATE FIRE MAINS REQUIRE A PERMIT FROM THE OFFICE OF CODE ADMINISTRATION...
8. AN AS-BUILT PLAN MUST BE SUBMITTED WITHIN 30 DAYS AFTER COMPLETION OF ALL CONSTRUCTION...
9. TEMPORARY STRUCTURES, CONSTRUCTION TRAILERS AND DEMOLITION REQUIRE PERMITS FROM THE OFFICE OF CODE ADMINISTRATION...
10. ADEQUATE EMERGENCY VEHICLE ACCESS SHALL BE MAINTAINED AT ALL TIMES...
11. CITY ORDINANCE PERMITS CONSTRUCTION NOISE, INCLUDING EXCAVATION, BETWEEN THE HOURS OF 7:00 AM AND 6:00 PM ON WEEKDAYS...
12. THE DEVELOPER SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH CITY CODE SECTIONS LIMITING GROWTH OF GRASS AND WEEDS TO SIX INCHES IN HEIGHT.

ENVIRONMENTAL

- 1. ALL EROSION SILTATION CONTROL TO BE INSTALLED PRIOR TO STARTING PROJECT TO CONFORM TO THE CURRENT VIRGINIA EROSION AND SEDIMENT CONTROL MANUAL...
2. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS OF CLEANING MUD FROM TRUCKS AND/OR OTHER EQUIPMENT PRIOR TO ENTERING THE CITY OF FAIRFAX RIGHTS-OF-WAY...
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM THE WORK IN SUCH A MANNER TO PREVENT THE WASHING OF ANY TOPSOIL, SILT, OR DEBRIS ONTO ADJACENT PROPERTIES...
4. IF THE PRESENCE OF ASBESTOS IS SUSPECTED IN THE SOIL, THE CONTRACTOR MUST CONTACT THE AIR POLLUTION CONTROL DIVISION...
5. ONSITE STORAGE OF FUEL SHALL BE LIMITED TO DIESEL FUEL TANKS NOT OVER 660 GALLONS CAPACITY...
6. ONSITE REPAIR OF VEHICLES AND EQUIPMENT SHALL BE LIMITED TO REPLACEMENT OF DAMAGED BELTS, HOSES AND TIRES...
7. THE OWNER SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH CITY CODE SECTIONS REGARDING HEALTH AND SAFETY MENAGES...
8. THE LINK TO THE ASBESTOS INFORMATION AND MAP ON THE FAIRFAX COUNTY WEBSITE...
9. PRIOR TO THE START OF ANY SITE GRADING WORK, THE DEVELOPER OR OWNER SHALL PROVIDE THE CITY OF FAIRFAX PUBLIC WORKS FACILITIES INSPECTOR WITH DOCUMENTATION THAT A VSPM PERMIT HAS BEEN ISSUED BY THE VA DEPARTMENT OF CONSERVATION AND RECREATION...
10. THE CONTRACTOR SHALL REQUEST PRE-CONSTRUCTION MEETING AND INSPECTION BY THE CITY OF FAIRFAX DEPARTMENT OF PUBLIC WORKS...
11. A PERMIT FOR INSTALLATION OF SANITARY SEWERS, FIRE HYDRANTS AND WATER MAINS SHALL BE OBTAINED FROM THE DEPARTMENT OF PUBLIC WORKS...
12. PRIOR TO ANY WATER MAIN INSTALLATION, ALL REQUIRED SANITARY SEWERS, INCLUDING LATERALS, AND STORM SEWERS MUST BE INSTALLED AND BACKFILLED TO 95% COMPACTION...
13. WATER AND SEWER LATERALS NOT WITHIN AN EASEMENT REQUIRE PLUMBING PERMITS AND INSPECTIONS FROM THE OFFICE OF CODE ADMINISTRATION...
14. FINAL APPROVAL FROM THE CITY OF FAIRFAX DEPARTMENT OF UTILITIES IS CONTINGENT UPON RECEIPT BY THE DEPARTMENT OF PLANS SHOWING THE LOCATION OF ALL PROPOSED UTILITIES...

DEPARTMENT OF UTILITIES STANDARD NOTES

GENERAL

- 1. ALL WATER MAINS AND SANITARY SEWERS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CURRENT CITY OF FAIRFAX STANDARDS AND SPECIFICATIONS...
2. EASEMENTS FOR ALL SANITARY AND WATER MAINS SHALL BE 10' UNLESS OTHERWISE NOTED...
3. SANITARY SEWERS AND WATER MAINS SHALL MAINTAIN A MINIMUM OF 15' HORIZONTAL SEPARATION FROM PROPOSED OR EXISTING BUILDINGS...
4. NO LANDSCAPING OR OTHER UTILITIES (I.E. GAS, PHONE, CABLE, ETC.) ARE PERMITTED IN THE WATER AND SEWER EASEMENTS, EXCEPT AT CROSSINGS...
5. CONTRACTOR SHALL REQUEST PRE-CONSTRUCTION MEETING AND INSPECTION BY THE CITY OF FAIRFAX DEPARTMENT OF PUBLIC WORKS...
6. A PERMIT FOR INSTALLATION OF SANITARY SEWERS, FIRE HYDRANTS AND WATER MAINS SHALL BE OBTAINED FROM THE DEPARTMENT OF PUBLIC WORKS...
7. PRIOR TO ANY WATER MAIN INSTALLATION, ALL REQUIRED SANITARY SEWERS, INCLUDING LATERALS, AND STORM SEWERS MUST BE INSTALLED AND BACKFILLED TO 95% COMPACTION...
8. WATER AND SEWER LATERALS NOT WITHIN AN EASEMENT REQUIRE PLUMBING PERMITS AND INSPECTIONS FROM THE OFFICE OF CODE ADMINISTRATION...
9. FINAL APPROVAL FROM THE CITY OF FAIRFAX DEPARTMENT OF UTILITIES IS CONTINGENT UPON RECEIPT BY THE DEPARTMENT OF PLANS SHOWING THE LOCATION OF ALL PROPOSED UTILITIES...

VDOT STANDARD CONSTRUCTION NOTES

- 1. ALL WORK ON THIS PROJECT SHALL CONFORM TO THE CURRENT EDITIONS OF AND LATEST REVISIONS TO THE VIRGINIA DEPARTMENT OF TRANSPORTATION (VDOT) ROAD AND BRIDGE SPECIFICATIONS AND STANDARDS...
2. ALL CONSTRUCTION SHALL COMPLY WITH THE LATEST US DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA), AND VIRGINIA OCCUPATIONAL SAFETY & HEALTH (VOSH) RULES AND REGULATIONS...
3. WHEN WORKING WITHIN VDOT RIGHT-OF-WAY, ALL TRAFFIC CONTROL, WHETHER PERMANENT OR TEMPORARY, SHALL BE IN ACCORDANCE WITH CURRENT EDITION OF VDOT'S WORK AREA PROTECTION MANUAL...
4. THE DEVELOPER SHALL BE RESPONSIBLE FOR RELOCATING, AT HIS EXPENSE, ANY AND ALL UTILITIES, INCLUDING TRAFFIC SIGNAL POLES, JUNCTION BOXES, CONTROLLERS, ETC., OWNED BY VDOT OR PRIVATE/PUBLIC UTILITY COMPANIES...
5. DESIGN FEATURES RELATING TO FIELD CONSTRUCTION, REGULATIONS, AND CONTROL OR SAFETY OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY VDOT...
6. PRIOR TO INITIATION OF WORK, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL NECESSARY VDOT OR LOCAL LAND USE PERMITS...
7. A PRE-CONSTRUCTION CONFERENCE SHALL BE ARRANGED AND HELD BY THE ENGINEER AND/OR DEVELOPER WITH THE ATTENDANCE OF THE CONTRACTOR...
8. THE CONTRACTOR SHALL NOTIFY THE COUNTY OFFICE WHEN WORK IS TO BEGIN OR CEASE...
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ADEQUATE ACCESS TO THE PROJECT FROM THE ADJACENT PUBLIC ROADWAY...
10. CONTRACTOR SHALL ENSURE ADEQUATE DRAINAGE IS ACHIEVED AND MAINTAINED ON THE SITE DURING AND AT THE END OF CONSTRUCTION...
11. ALL WATER AND SEWER LINES WITHIN EXISTING OR PROPOSED VDOT RIGHT-OF-WAY SHALL HAVE A MINIMUM THIRTY SIX (36) INCHES COVER...
12. ANY UNUSUAL SUBSURFACE CONDITIONS (E.G., UNSUITABLE SOILS, SPRINGS, SINKHOLES, VOIDS, CAVES, ETC.) ENCOUNTERED DURING THE COURSE OF CONSTRUCTION SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER AND VDOT...
13. ALL FILL AREAS, BORROW MATERIAL AND UNDERCUT AREAS SHALL BE INSPECTED AND APPROVED BY A VDOT REPRESENTATIVE...
14. ALL ROADWAY FILL, BASE, SUBGRADE MATERIAL, AND BACKFILL IN UTILITY /STORM SEWER TRENCHES SHALL BE COMPACTED IN SIX (6) INCH LIFTS...
15. VDOT STANDARD CD AND UD UNDERDRAINS SHALL BE INSTALLED WHERE INDICATED ON THESE PLANS AND/OR AS SPECIFIED BY VDOT...
16. THE INSTALLATION OF ANY ENTRANCES AND MAILBOXES WITHIN ANY DEDICATED STREET RIGHT-OF-WAY SHALL MEET THE VDOT MINIMUM DESIGN STANDARDS...
17. PRIOR TO VDOT ACCEPTANCE OF ANY STREETS, ALL REQUIRED STREET SIGNAGE AND/OR PAVEMENT MARKINGS SHALL BE INSTALLED...
18. THE DEVELOPER SHALL PROVIDE THE VDOT RESIDENCY OFFICE WITH A LIST OF ALL MATERIAL SOURCES PRIOR TO THE START OF CONSTRUCTION...
19. AGGREGATE BASE AND SUBBASE MATERIALS SHALL BE PLACED ON SUBGRADE BY MEANS OF A MECHANICAL SPREADER...
20. ASPHALT CONCRETE PAVEMENTS SHALL BE PLACED IN ACCORDANCE WITH SECTION 315 OF THE ROAD AND BRIDGE SPECIFICATIONS...
21. IN ACCORDANCE WITH SECTION 302.3, THE FOUNDATIONS FOR PIPE CULVERTS THIRTY SIX (36) INCHES AND LARGER SHALL BE EXPLORED BELOW THE BOTTOM OF THE EXCAVATION...
22. THE FOUNDATIONS FOR ALL BOX CULVERTS SHALL BE INVESTIGATED BY MEANS OF EXPLORATORY BORINGS ADVANCED BELOW PROPOSED FOUNDATION ELEVATION...

- BOX. FOUNDATION DESIGNS SHALL COMPLY WITH VDOT ROAD AND BRIDGE STANDARD PB-1...
23. APPROVAL OF THESE PLANS SHALL EXPIRE THREE (3) YEARS FROM THE DATE OF THE APPROVAL LETTER...
24. VDOT STANDARD CG-12 CURB RAMPS SHALL BE INSTALLED WHERE INDICATED ON THESE PLANS AND/OR AS SPECIFIED BY VDOT...
25. VDOT STANDARD GUARDRAIL SHALL BE INSTALLED WHERE WARRANTED AND/OR AS PROPOSED ON THESE PLANS...
13. ALL CONSTRUCTION SHALL COMPLY WITH THE LATEST US DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA), AND VIRGINIA OCCUPATIONAL SAFETY & HEALTH (VOSH) RULES AND REGULATIONS...
14. THE DEVELOPER SHALL BE RESPONSIBLE FOR RELOCATING, AT HIS EXPENSE, ANY AND ALL UTILITIES...
15. DESIGN FEATURES RELATING TO FIELD CONSTRUCTION, REGULATIONS, AND CONTROL OR SAFETY OF TRAFFIC MAY BE SUBJECT TO CHANGE...
16. PRIOR TO INITIATION OF WORK, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL NECESSARY VDOT OR LOCAL LAND USE PERMITS...
17. A PRE-CONSTRUCTION CONFERENCE SHALL BE ARRANGED AND HELD BY THE ENGINEER AND/OR DEVELOPER WITH THE ATTENDANCE OF THE CONTRACTOR...
18. THE CONTRACTOR SHALL NOTIFY THE COUNTY OFFICE WHEN WORK IS TO BEGIN OR CEASE...
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ADEQUATE ACCESS TO THE PROJECT FROM THE ADJACENT PUBLIC ROADWAY...
20. CONTRACTOR SHALL ENSURE ADEQUATE DRAINAGE IS ACHIEVED AND MAINTAINED ON THE SITE DURING AND AT THE END OF CONSTRUCTION...
21. ALL WATER AND SEWER LINES WITHIN EXISTING OR PROPOSED VDOT RIGHT-OF-WAY SHALL HAVE A MINIMUM THIRTY SIX (36) INCHES COVER...
22. ANY UNUSUAL SUBSURFACE CONDITIONS (E.G., UNSUITABLE SOILS, SPRINGS, SINKHOLES, VOIDS, CAVES, ETC.) ENCOUNTERED DURING THE COURSE OF CONSTRUCTION SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER AND VDOT...
23. ALL FILL AREAS, BORROW MATERIAL AND UNDERCUT AREAS SHALL BE INSPECTED AND APPROVED BY A VDOT REPRESENTATIVE...
24. ALL ROADWAY FILL, BASE, SUBGRADE MATERIAL, AND BACKFILL IN UTILITY /STORM SEWER TRENCHES SHALL BE COMPACTED IN SIX (6) INCH LIFTS...
25. VDOT STANDARD CD AND UD UNDERDRAINS SHALL BE INSTALLED WHERE INDICATED ON THESE PLANS AND/OR AS SPECIFIED BY VDOT...
26. THE INSTALLATION OF ANY ENTRANCES AND MAILBOXES WITHIN ANY DEDICATED STREET RIGHT-OF-WAY SHALL MEET THE VDOT MINIMUM DESIGN STANDARDS...
27. PRIOR TO VDOT ACCEPTANCE OF ANY STREETS, ALL REQUIRED STREET SIGNAGE AND/OR PAVEMENT MARKINGS SHALL BE INSTALLED...
28. THE DEVELOPER SHALL PROVIDE THE VDOT RESIDENCY OFFICE WITH A LIST OF ALL MATERIAL SOURCES PRIOR TO THE START OF CONSTRUCTION...
29. AGGREGATE BASE AND SUBBASE MATERIALS SHALL BE PLACED ON SUBGRADE BY MEANS OF A MECHANICAL SPREADER...
30. ASPHALT CONCRETE PAVEMENTS SHALL BE PLACED IN ACCORDANCE WITH SECTION 315 OF THE ROAD AND BRIDGE SPECIFICATIONS...
31. IN ACCORDANCE WITH SECTION 302.3, THE FOUNDATIONS FOR PIPE CULVERTS THIRTY SIX (36) INCHES AND LARGER SHALL BE EXPLORED BELOW THE BOTTOM OF THE EXCAVATION...
32. THE FOUNDATIONS FOR ALL BOX CULVERTS SHALL BE INVESTIGATED BY MEANS OF EXPLORATORY BORINGS ADVANCED BELOW PROPOSED FOUNDATION ELEVATION...

ADDITIONAL VDOT CONSTRUCTION NOTES

- 1. METHODS AND MATERIALS USED SHALL CONFORM TO CURRENT CITY AND VDOT STANDARDS AND SPECIFICATIONS...
2. ALL UTILITIES, INCLUDING ALL POLES, ARE TO BE RELOCATED AT THE CONTRACTORS EXPENSE...
3. OPEN CUTTING OF PAVED OR SURFACE TREATED ROADS IS NOT PERMITTED...
4. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO EXISTING ROADS AND UTILITIES WHICH OCCUR AS A RESULT OF PROJECT CONSTRUCTION...
5. A SMOOTH GRADE SHALL BE MAINTAINED FROM THE CENTERLINE OF THE EXISTING ROAD TO PROPOSED EDGE OF PAVEMENT...
6. STANDARD GUARDRAILS AND/OR HANDRAILS SHALL BE INSTALLED AT HAZARDOUS LOCATIONS...
7. THE CONTRACTOR IS RESPONSIBLE FOR ALL TRAFFIC CONTROL...
8. CBR TEST SHALL BE PERFORMED PRIOR TO DETERMINATION OF FINAL SUBGRADE ELEVATION...
9. A 4" (MIN.) LAYER OF STONE IS REQUIRED BENEATH CURB AND GUTTER...
10. OVERLAY OF EXISTING PAVEMENT SHALL BE MINIMUM OF 1.5" DEPTH...
11. OWNER IS RESPONSIBLE FOR DESIGN AND CONSTRUCTION OF ANY TRAFFIC SIGNAL INSTALLATION...
12. LANDSCAPING AND IRRIGATION SYSTEMS SHALL NOT BE INSTALLED WITHIN THE PUBLIC RIGHT OF WAY...
13. ALL RIGHT OF WAY DEDICATED TO PUBLIC USE SHALL BE CLEAR AND UNOCCUPIED...
14. THE OWNER SHALL OBTAIN A PERMIT FOR ALL SIDEWALKS WITHIN THE RIGHT OF WAY...
15. TRAFFIC CONTROL DEVICES OR ADVISORY SIGNS, SUCH AS MULTIWAY STOPS, SPEED LIMITS, DEAF CHILD, CHILDREN AT PLAY, ETC., SHALL NOT BE INSTALLED UNLESS SPECIFICALLY SHOWN ON THESE PLANS...
16. LANDSCAPING AND IRRIGATION SYSTEMS SHALL NOT BE INSTALLED WITHIN THE PUBLIC RIGHT OF WAY EXCEPT AS SHOWN ON THESE PLANS...

STORM SEWER CONSTRUCTION NOTES

- 1. ALL CONSTRUCTION AND MATERIALS SHALL CONFORM WHERE APPLICABLE TO THE CURRENT VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE SPECIFICATIONS...
2. ALL CONCRETE SHALL BE CLASS A3 IF CAST IN PLACE, CLASS A4 IF PRECAST...
3. MANHOLES AND DROP INLETS SHALL BE CONSTRUCTED FROM INVERT TO TOP AS FOLLOWS:
A. MANHOLES TO EIGHT FEET DEEP: BLOCK CONSTRUCTION - MINIMUM EIGHT INCH WALLS.
POURED IN PLACE CONCRETE - MINIMUM EIGHT INCH WALLS AND NONREINFORCED.
PRECAST - MINIMUM EIGHT INCH WALLS IN CONJUNCTION WITH PRECAST THROAT AND PRECAST BASE SLAB.
B. MANHOLES OVER EIGHT FEET DEEP.
PRECAST.
POURED IN PLACE REINFORCED CONCRETE.
SPECIAL DESIGN, I.E. BENDS, PRECAST TEES, PRECAST BOXES, WEYS.
4. DROP INLETS AND CURB INLETS SHALL HAVE STEPS...
5. UNLESS STATED ON THE APPROVED PLANS, SYMMETRICAL CHANNELS SHALL BE PERFORMED IN THE INVERT OF ALL STRUCTURES...
6. IF BLOCK CONSTRUCTION IS USED, THE INSIDE AND OUTSIDE WALLS, AS THEY ARE LAID, SHALL BE PLASTERED WITH MORTAR...
7. ALL PRECAST DROP INLETS, CURB INLETS AND MANHOLES SHALL CONFORM TO ASTM C-478...
8. VDOT INLETS, WHERE PIPE SIZE IS LARGER THAN 48 INCHES I.D., REQUIRE A SPECIAL DESIGN...
9. THE OPENING IN PRECAST STORM SEWER STRUCTURES FOR ALL SIZE PIPE SHALL BE A MINIMUM OF FOUR INCHES AND A MAXIMUM OF SIX INCHES LARGER THAN THE OUTSIDE DIAMETER OF THE PIPE...
10. THE "H" DIMENSIONS SHOWN ON THE STANDARDS AND SPECIFIED ON THE PLANS WILL BE MEASURED FROM THE INVERT OF OUTFALL PIPE...
11. TWO (2) INCH DEEP HOLES SHALL BE PROVIDED IN ENDWALL WHERE DIRECTED BY THE INSPECTOR...
12. ALL PIPES ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE...
13. ALL FILL BENEATH SEWER PIPES AND WATERLINES IS TO BE CONTROLLED FILL OR BETTER.

BETTER. CONTROLLED FILLS MUST BE COMPACTED TO 100% DENSITY AS DETERMINED BY AASHTO T99 OR ASTM 4-698...
14. ALL FILL BENEATH MANHOLES IS TO BE SELECT FILL. SELECT FILL MATERIAL SHALL CONSIST OF #67 OR #78 STONE AND MUST BE COMPACTED TO 100% DENSITY AS DETERMINED BY AASHTO T99 OR ASTM 4-698...

MATERIAL NOTES:

- 1. ALL CONCRETE PIPE SHALL BE CLASS III OR AS NOTED ON THE PLANS...
2. ALL WATERLINES LARGER THAN 4" SHALL BE DIP CLASS 52 OR AS REQUIRED BY FAIRFAX WATER...
3. ALL WATER SERVICE PIPE LESS THAN 4" FOR CONNECTION FROM THE MAIN TO THE METER SHALL BE TYPE "K" COOPER OR AS REQUIRED PER FAIRFAX WATER...
4. ALL MANHOLES SHALL BE OF WATER TIGHT CONSTRUCTION AND TESTED IN PLACE PER VIRGINIA WATERWORKS REGULATIONS 12 VAC 5-590-1150. 132C, PAGE 146, DATED NOVEMBER 15, 1995.

THIS DRAWING PREPARED AT THE NORTHERN VIRGINIA OFFICE 701 E. Ashby Ave. Suite 1001 Ashburn, VA 2047 1-703-282-1242 FAX 703-282-1245 WWW.TIMMONSGROUP.COM REVISION DESCRIBED HEREIN

DATE 09.22.2007

DESIGNED BY CMK CHECKED BY CMK SCALE SHEET PLAN



JUDICIAL DRIVE TRAIL CONNECTION CITY OF FAIRFAX, VIRGINIA GENERAL NOTES

JOB NO. 35061.030 SHEET NO. C1.0

# #

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r:\2006\35061\_030-judicial drive trail\DWG Sheet\CDC2.0-SURVEY.dwg | Plotted on 9/22/2017 2:35 PM | by Casey Kight

N/F CITY OF FAIRFAX  
DB. 7158, PG. 115  
PCL. ID. #57-1-29-B-001-A

TOP=389.80  
INV. OUT=375.60  
(FULL OF DEBRIS)

TOP=389.81  
8" INV. IN=373.70(A)  
2" INV. IN=372.91(B)  
2" INV. OUT=372.68

N/F PRESBYTERY OF WASHINGTON  
DB. 1228, PG. 486  
PCL. ID. #57-1-02-123

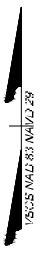
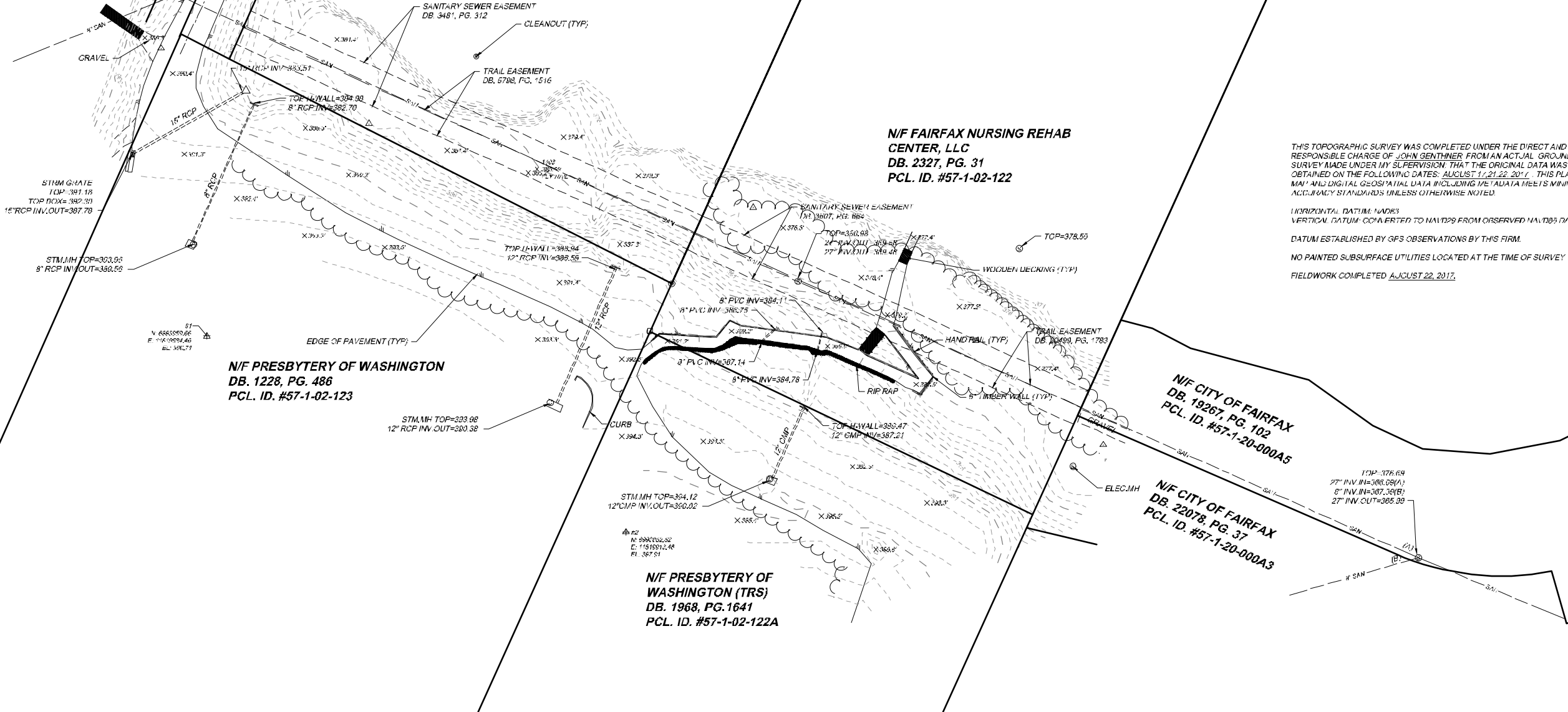
N/F ONE CAPITAL PARTNERS, LLC  
DB. 9685, PG. 685  
PCL. ID. #57-1-02-124

N/F FAIRFAX NURSING REHAB  
CENTER, LLC  
DB. 2327, PG. 31  
PCL. ID. #57-1-02-122

N/F PRESBYTERY OF  
WASHINGTON (TRS)  
DB. 1968, PG. 1641  
PCL. ID. #57-1-02-122A

N/F CITY OF FAIRFAX  
DB. 19267, PG. 102  
PCL. ID. #57-1-20-000A5

N/F CITY OF FAIRFAX  
DB. 22078, PG. 37  
PCL. ID. #57-1-20-000A3



THIS TOPOGRAPHIC SURVEY WAS COMPLETED UNDER THE DIRECT AND RESPONSIBLE CHARGE OF JOHN GENTNER FROM AN ACTUAL GROUND SURVEY MADE UNDER MY SUPERVISION. THAT THE ORIGINAL DATA WAS OBTAINED ON THE FOLLOWING DATES: AUGUST 17, 21, 22, 2017. THIS PLAN, MAP AND DIGITAL GEOSPATIAL DATA INCLUDING METADATA MEETS MINIMUM ACCURACY STANDARDS UNLESS OTHERWISE NOTED.

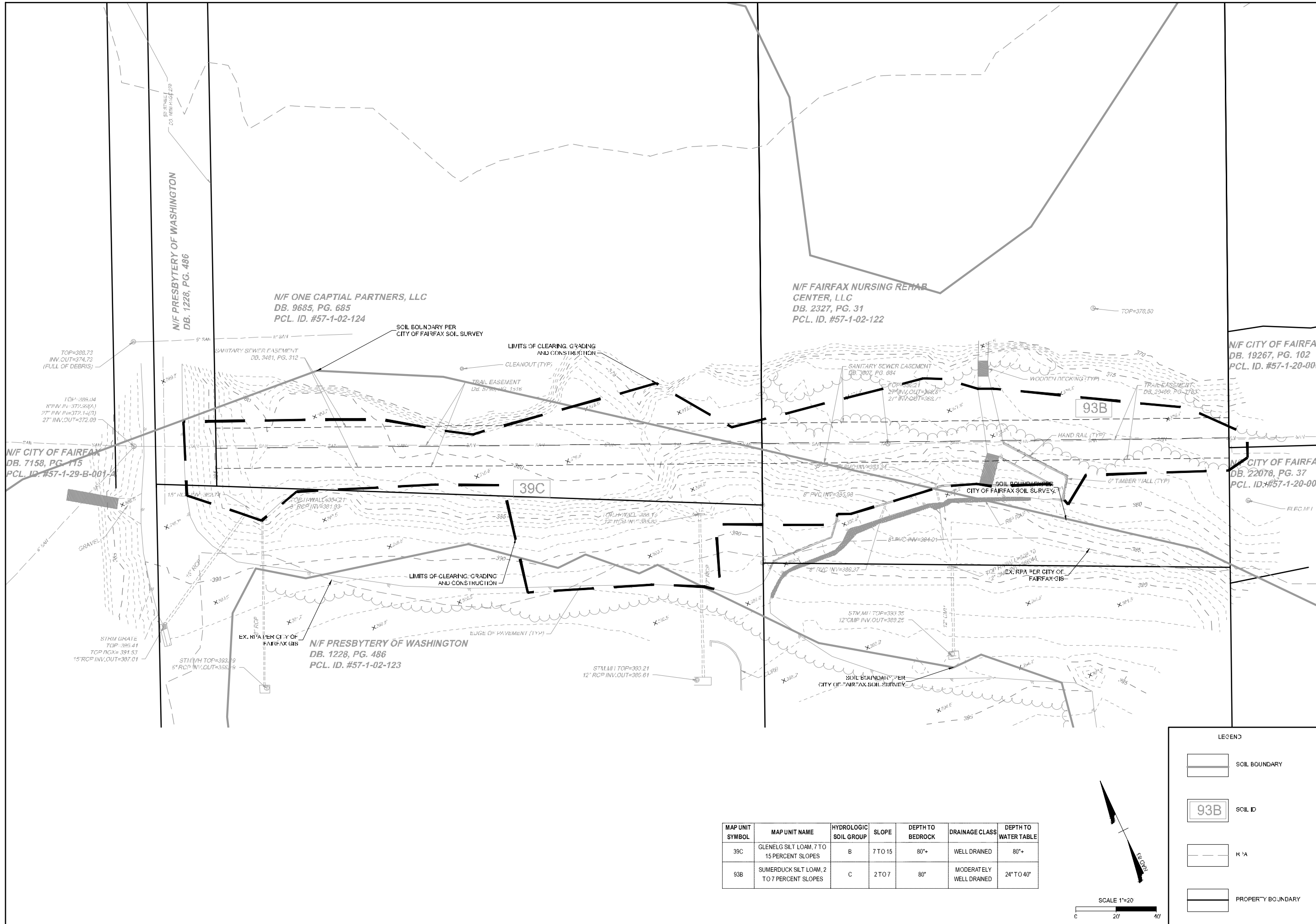
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VERTICAL DATUM: CONVERTED TO NAVD83 FROM OBSERVED NAVD83 DATUM ESTABLISHED BY GPS OBSERVATIONS BY THIS FIRM.

NO PAINTED SUBSURFACE UTILITIES LOCATED AT THE TIME OF SURVEY  
FIELDWORK COMPLETED AUGUST 22, 2017.

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DATE	AUG. 21 2017
DRAWN BY	TJ EAST F
CHECKED BY	J. GENTNER
SCALE	1"=30'
<b>TIMMONS GROUP</b> JUDICIAL DRIVE TRAIL CONNECTION CITY OF FAIRFAX, VIRGINIA SURVEY	
PROJECT NO.	35061.030
SHEET NO.	51.0

TOPOGRAPHICAL SURVEY  
ON THE PROPERTY OF  
THE CITY OF FAIRFAX  
JUDICIAL SQUARE

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MAP UNIT SYMBOL	MAP UNIT NAME	HYDROLOGIC SOIL GROUP	SLOPE	DEPTH TO BEDROCK	DRAINAGE CLASS	DEPTH TO WATER TABLE
39C	GLENELG SILT LOAM, 7 TO 15 PERCENT SLOPES	B	7 TO 15	80"+	WELL DRAINED	80"+
93B	SUMERDUCK SILT LOAM, 2 TO 7 PERCENT SLOPES	C	2 TO 7	80"	MODERATELY WELL DRAINED	24" TO 40"

**LEGEND**

- SOIL BOUNDARY
- SOIL ID
- EASEMENT
- PROPERTY BOUNDARY

SCALE 1"=20'

DATE

09.22.2017

DESIGNED BY CMK

CHECKED BY CMK

SCALE SEE PLAN

**TIMMONS GROUP**

JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA

EXISTING CONDITIONS

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REVISION DESCR. P11021

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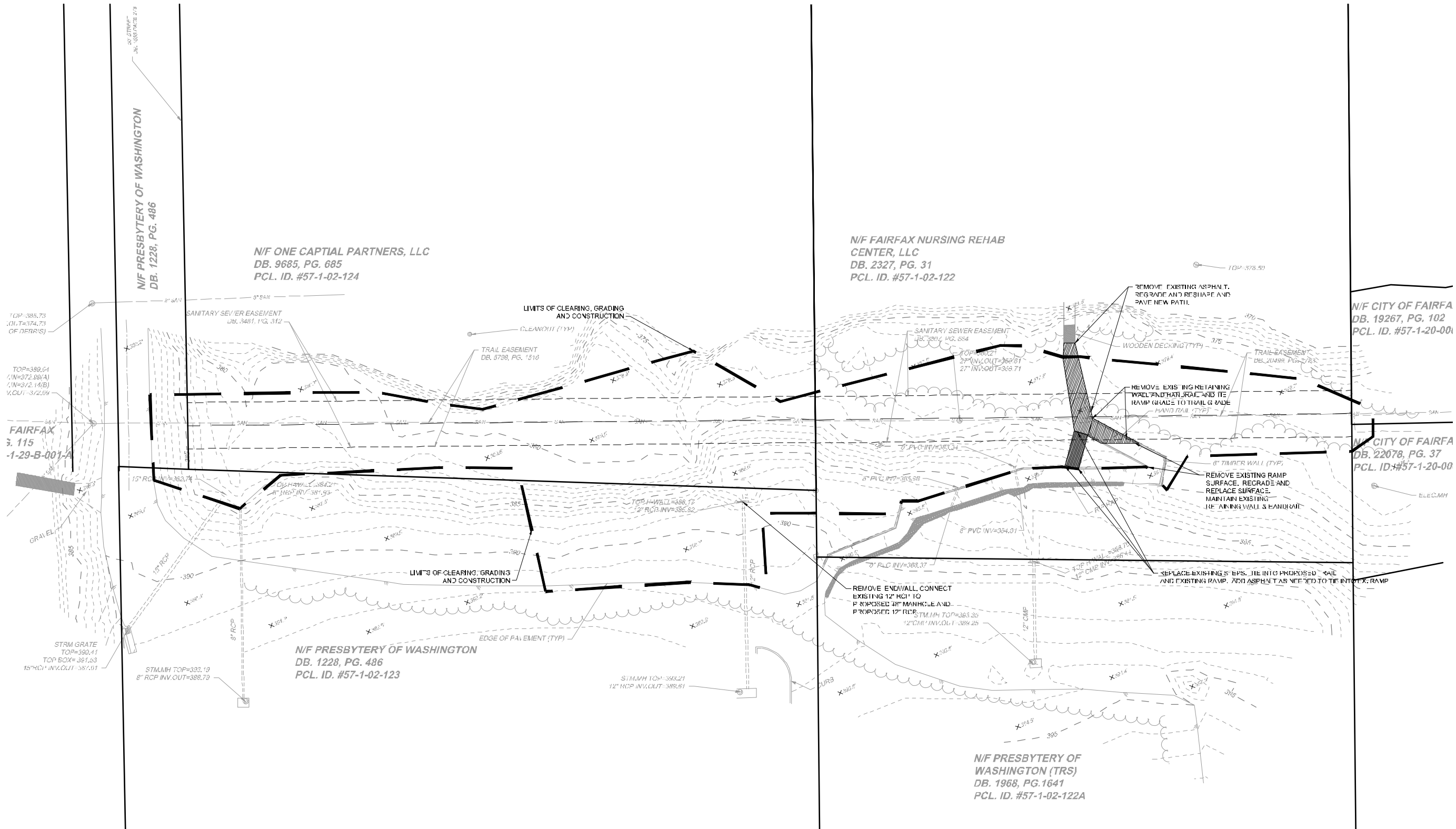
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SCALE SEE PLAN

JOB NO.: 35061.030

SHEET NO.: C3.0

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N/F PRESBYTERY OF WASHINGTON  
DB. 1228, PG. 486

N/F ONE CAPITAL PARTNERS, LLC  
DB. 9685, PG. 685  
PCL. ID. #57-1-02-124

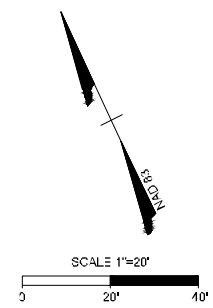
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CENTER, LLC  
DB. 2327, PG. 31  
PCL. ID. #57-1-02-122

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DB. 19267, PG. 102  
PCL. ID. #57-1-20-001

N/F CITY OF FAIRFA  
DB. 22078, PG. 37  
PCL. ID. #57-1-20-00

N/F PRESBYTERY OF WASHINGTON  
DB. 1228, PG. 486  
PCL. ID. #57-1-02-123

N/F PRESBYTERY OF  
WASHINGTON (TRS)  
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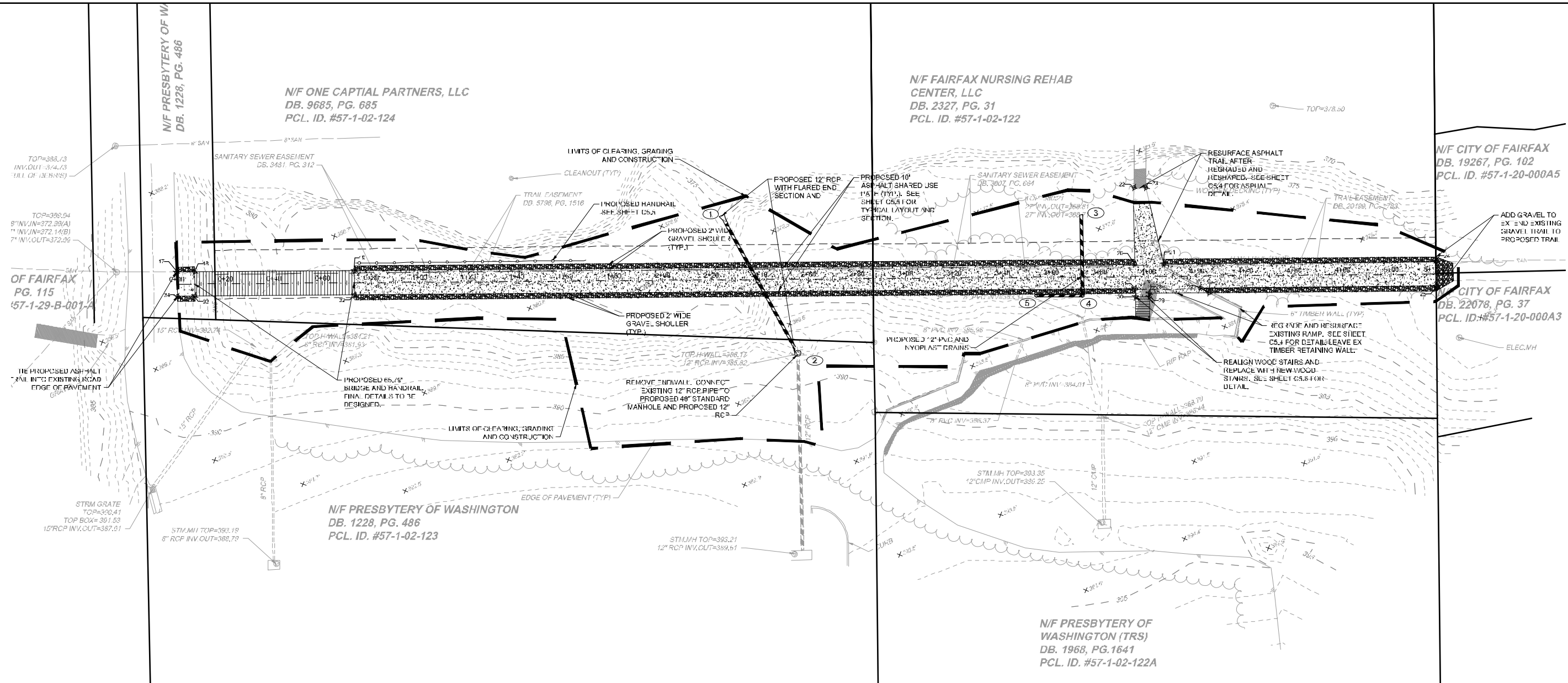
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JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
DEMOLITION PLAN

JOB NO.: 35061.030  
SHEET NO.: C4.0

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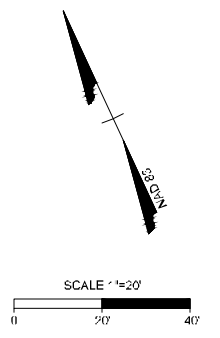
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REVISION	DESCRIBE REVISION

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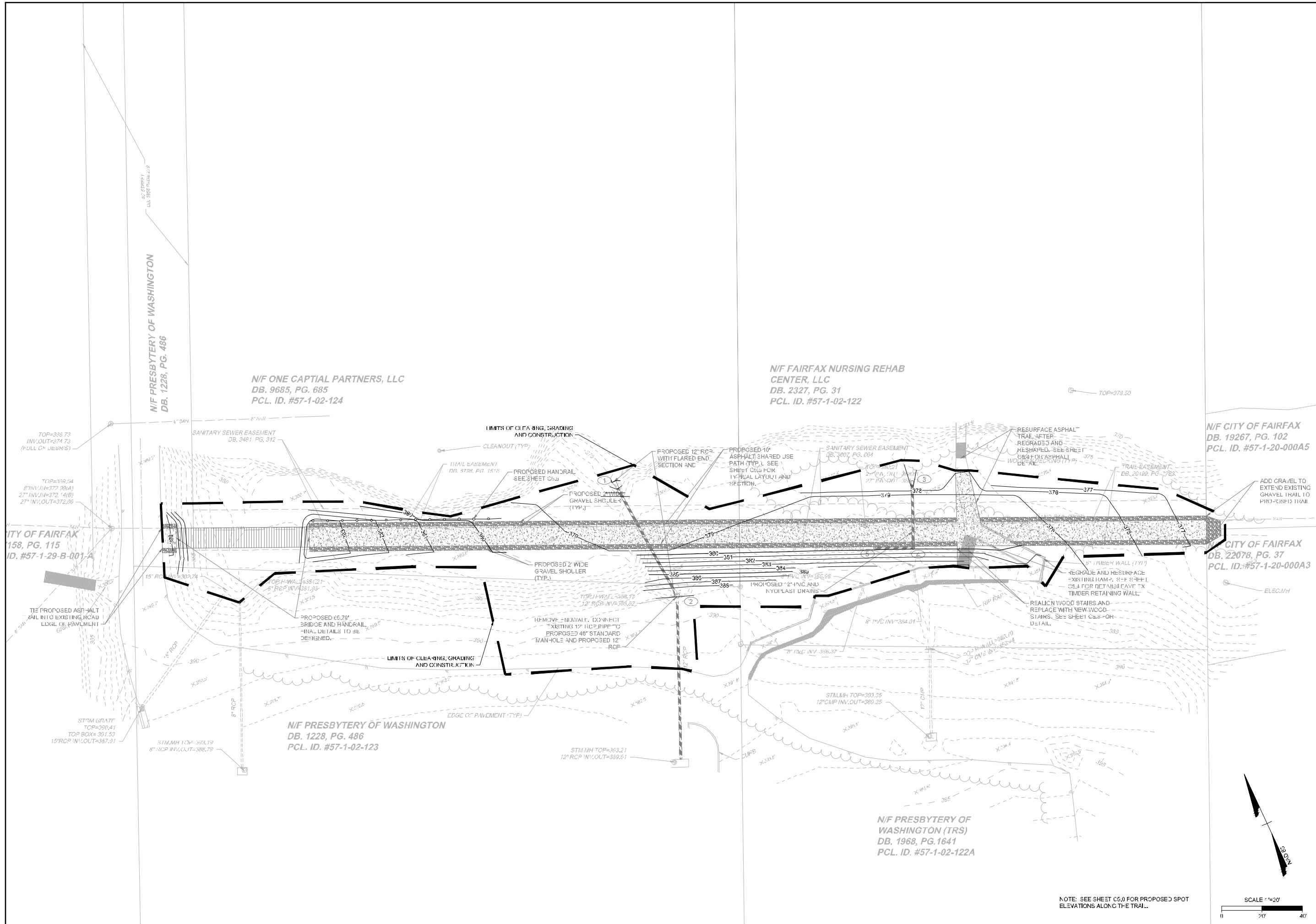
JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
LAYOUT AND GEOMETRY

JOB NO.: 35061.030  
SHEET NO.: C5.0

JUDICIAL DRIVE TRAIL GEOMETRY POINTS					
POINT NO.	STATION	STATION OFFSET	NORTHING	EASTING	ELEVATION
17	0+00.00	5'	6994136.03	11819690.44	389.42
18	0+07.28	5'	6994132.95	11819697.19	388.11
19	0+73.07	5'	6994105.64	11819757.04	383.00
20	3+94.44	5'	6993972.22	11820049.40	379.50
21	4+06.36	5'	6993967.27	11820060.24	379.49
22	3+94.38	36.56'	6994000.89	11820062.44	377.17
23	3+99.30	36.53'	6993998.82	11820086.90	377.12
24	5+18.47	5'	6993920.72	11820162.24	376.50
25	5+18.47	5'	6993911.62	11820158.10	0.00
26	4+21.15	5'	6993952.03	11820069.55	379.67
27	4+24.84	5'	6993948.68	11820072.07	379.98
28	4+00.37	6.93'	6993958.84	11820049.81	380.00
29	4+00.36	8.38'	6993957.53	11820049.20	380.00
30	3+94.61	8.38'	6993958.91	11820043.97	380.00
31	3+94.61	5'	6993963.05	11820045.40	379.87
32	0+73.07	5'	6994096.54	11819752.89	383.00
33	0+07.28	5'	6994123.86	11819693.04	388.37
34	0+00.00	5'	6994126.82	11819686.53	389.44



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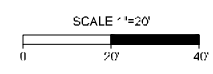
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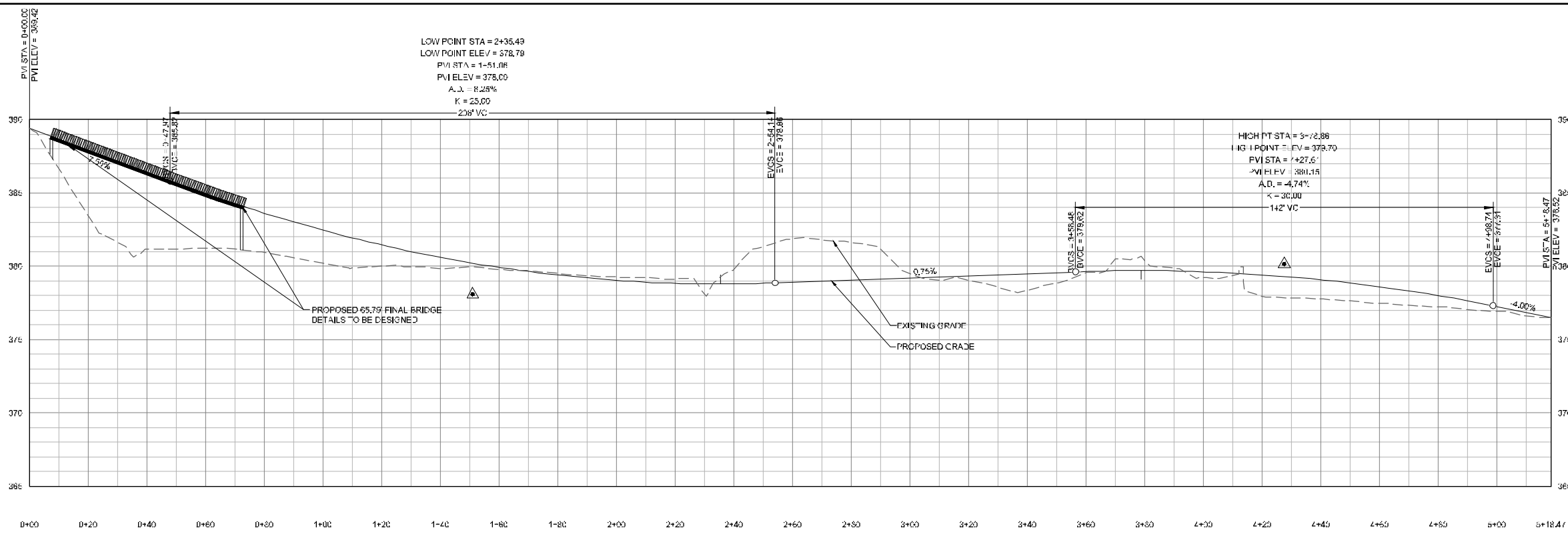
JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
**PROPOSED GRADING**

JOB NO.: 35061.030  
 SHEET NO.: C5.1

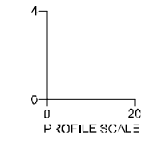
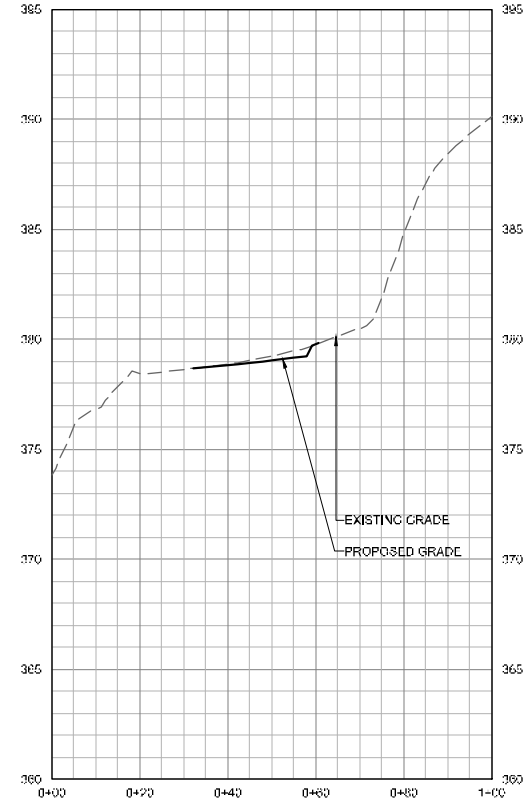
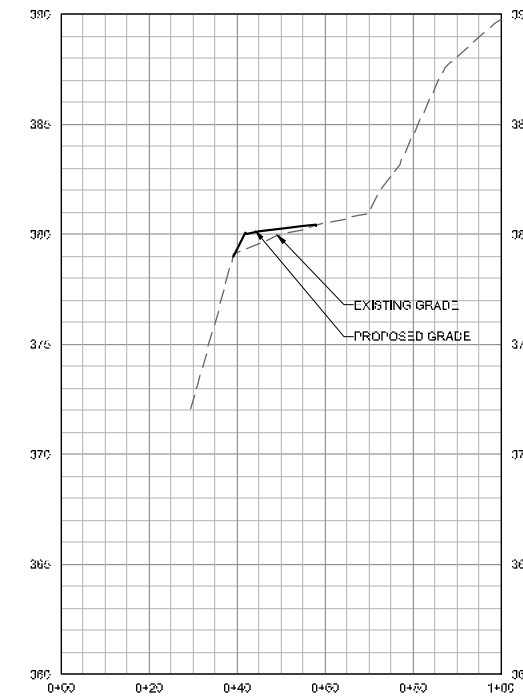
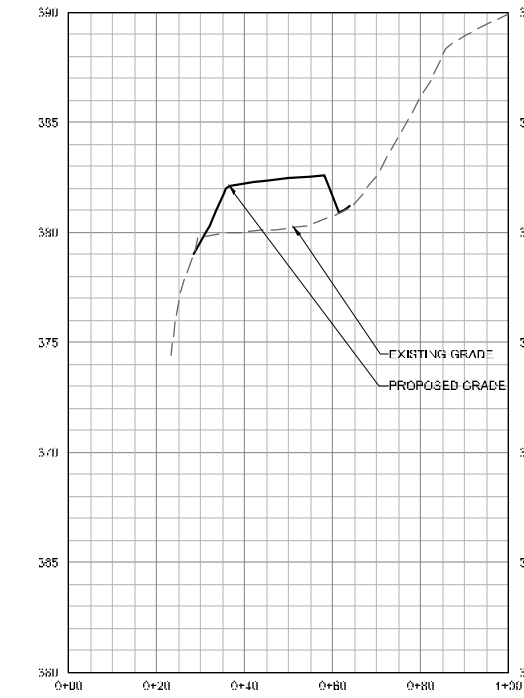
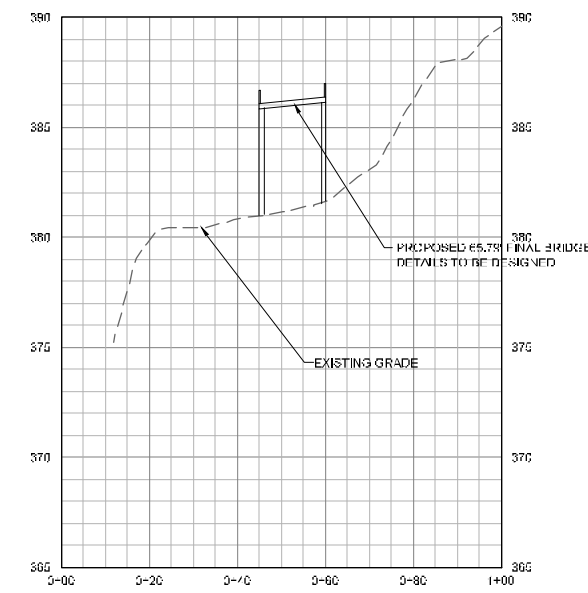
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NOTE: SEE SHEET C6.0 FOR PROPOSED SPOT ELEVATIONS ALONG THE TRAIL.





0+00 0+20 0+40 0+60 0+80 1+00 1+20 1+40 1+60 1+80 2+00 2+20 2+40 2+60 2+80 3+00 3+20 3+40 3+60 3+80 4+00 4+20 4+40 4+60 4+80 5+00 5+18.47



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SCALE	SFF PLAN

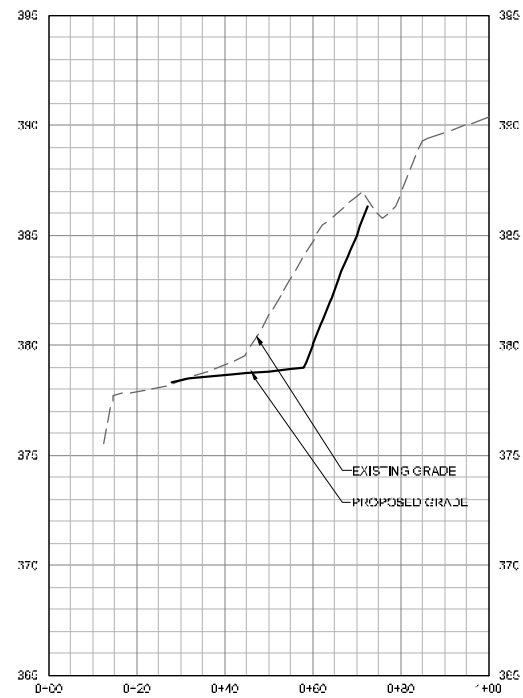
JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
 TRAIL PROFILE AND CROSS SECTIONS

JOB NO.:  
 35061.030  
 SHEET NO.:  
 C5.2

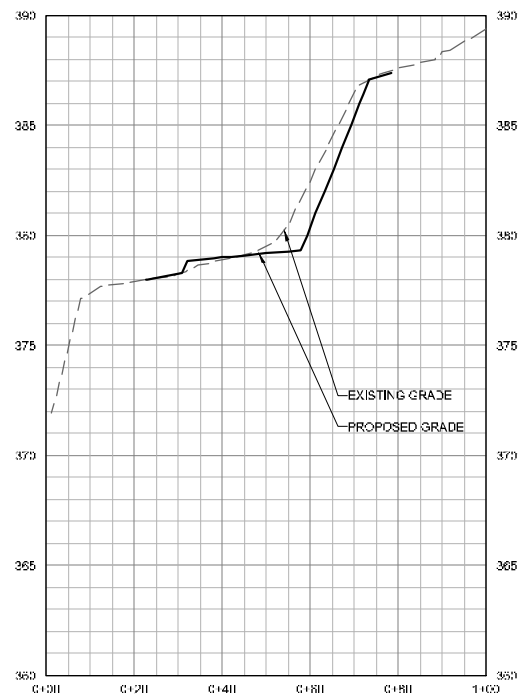


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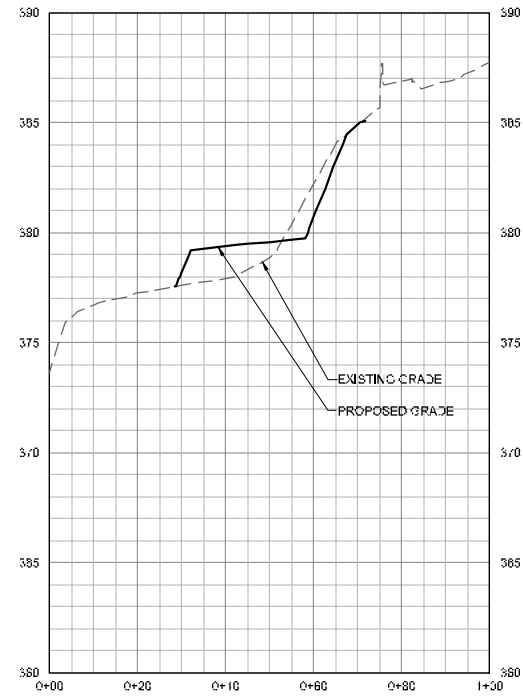




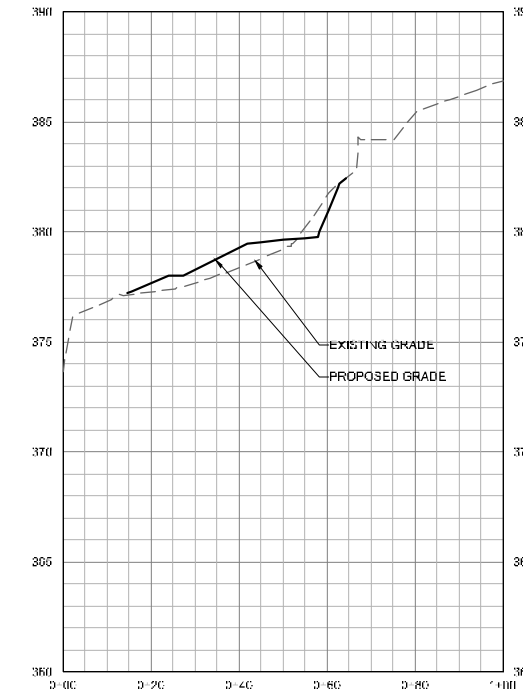
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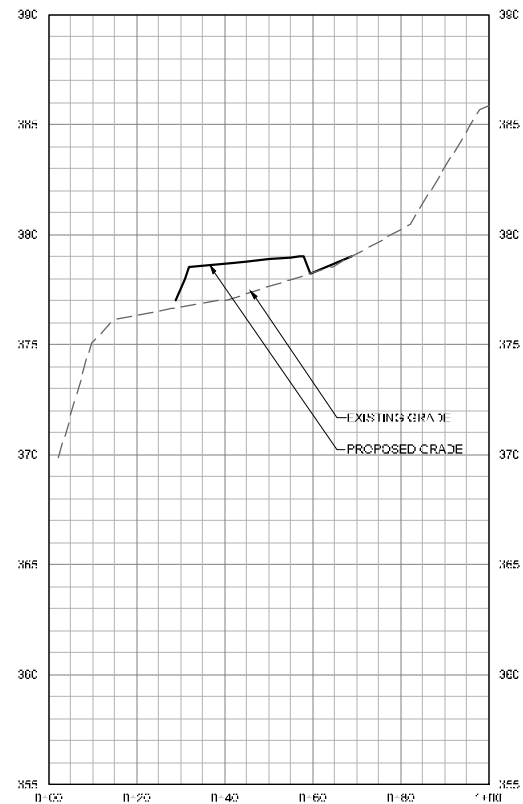
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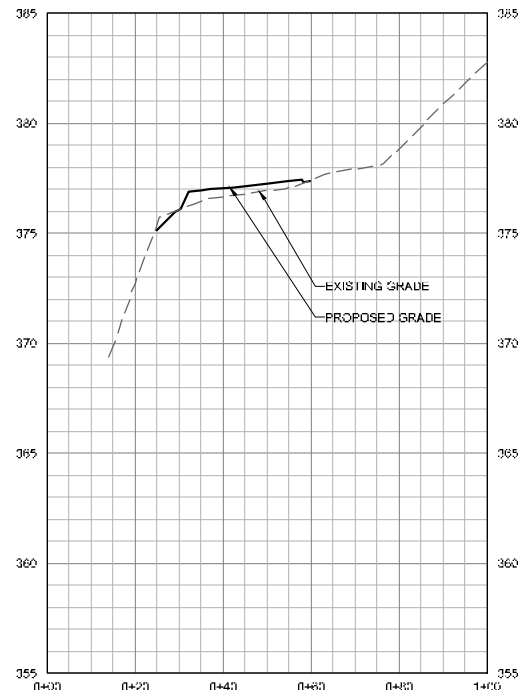
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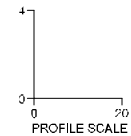
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PROFILE STATION 4+50.00  
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PROFILE STATION 5+00.00  
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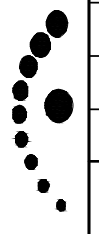
DATE	REVISION	DESCRIPTION
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CHK

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SCALE  
SEE PLAN



JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA

JOB NO.:  
35061.030

SHEET NO.:  
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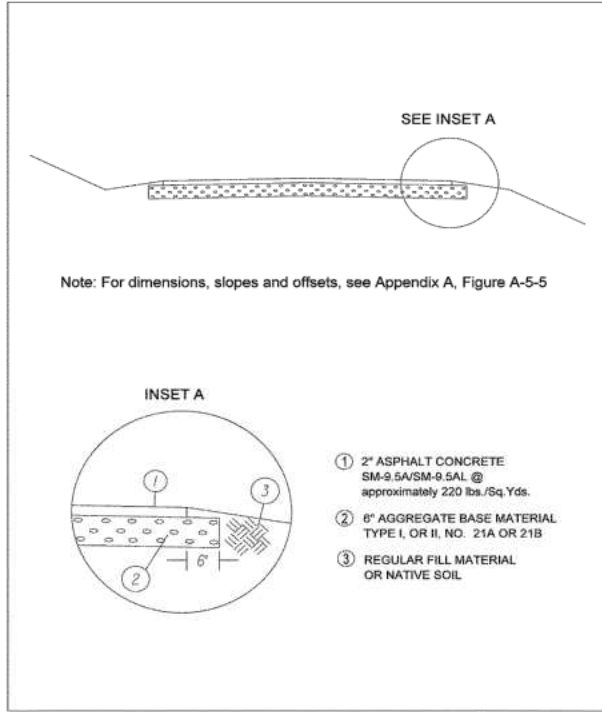


FIGURE A-5-6.1 SHARED USE PATH PAVEMENT STRUCTURE\*

\* Added 1/16

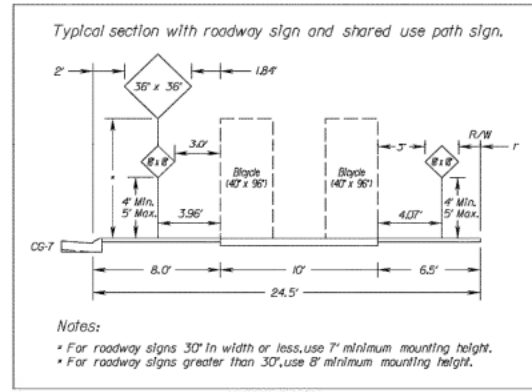


FIGURE A-5-4 SHARED USE PATHS

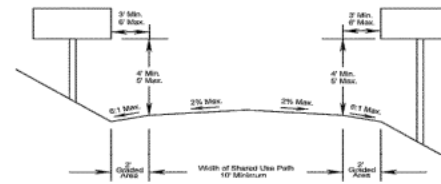
• Width and Horizontal Clearance

The paved width and the operating width required for a shared use path are primary design considerations. The minimum paved width for a two-directional shared use path is 10 feet. See Figure A-5-5. However in rare instances, a reduced width of 8 feet may be used. This reduced width may be used only where the following conditions prevail:

- Bicycle traffic is expected to be low, even on peak days or during peak hours.
- Pedestrian use of the facility is not expected to be more than occasional.
- There will be good horizontal and vertical alignment providing safe and frequent passing opportunities, and
- During normal maintenance activities the path will not be subjected to maintenance vehicle loading conditions that would cause pavement edge damage.

Under certain conditions it may be necessary or desirable to increase the width of a shared use path to 11 feet, or even 14 feet, due to substantial use by bicycles, joggers, skaters and pedestrians, use by large maintenance vehicles, and steep grades.

\* Rev. 7/13



The minimum width of a one-directional shared use path is 6 feet. A one-way path would rarely be designed and only in a special situation. It should be recognized that one-way paths often would be used as two-way facilities unless effective measures are taken to assure one-way operation. Without such enforcement, it should be assumed that shared use paths would be used as two-way facilities by both pedestrians and bicyclists and designed accordingly.

A minimum 2 foot wide graded area with a maximum 6:1 slope, shall be maintained adjacent to both sides of the path. A minimum 3 foot clearance shall be maintained from the edge of the path to signs, trees, poles, walls, fences, railing, guardrail, or other lateral obstructions. Where the path is adjacent to parallel water hazard, other obvious hazard or downward slope equal to or steeper than 3:1, a minimum 5 foot wide separation from the edge of the path pavement to the top of slope is required. When the separation from the edge of the shared use path to the top of the slope is less than 5 feet, a physical barrier such as railing (HR-1, Type III) or chain link fence is required in the following situations:

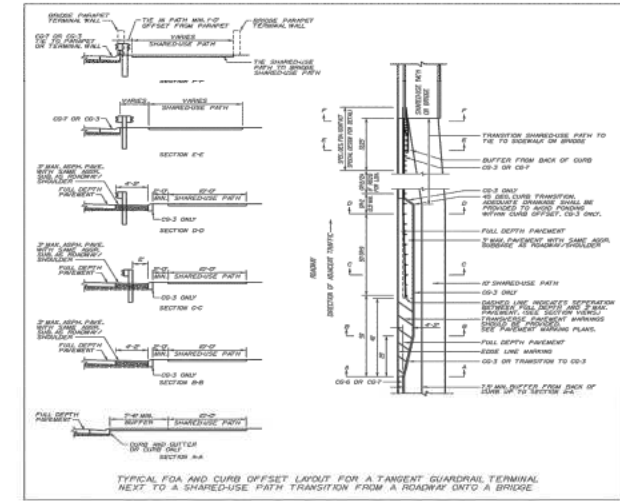
- Slopes 2:1 or steeper, with a drop of 4 feet or greater
- Slopes 3:1 or steeper, with a drop of 6 feet or greater
- Slopes 3:1 or steeper, adjacent to a parallel water hazard or other obvious hazard

See FIGURE A-5-6 PHYSICAL BARRIER FOR SHARED-USE PATH

Note: When the separation from the edge of the shared use path to the top of the slope is 5 feet or greater situations may dictate a physical barrier, such as the height of embankment or conditions at the bottom (i.e. - water greater than 2 feet deep).

When a shared use path is constructed adjacent to a retaining wall or a structure with a drop-off 1' or more, a railing or chain link fence 54 inches high is required.

\* Rev. 1/14

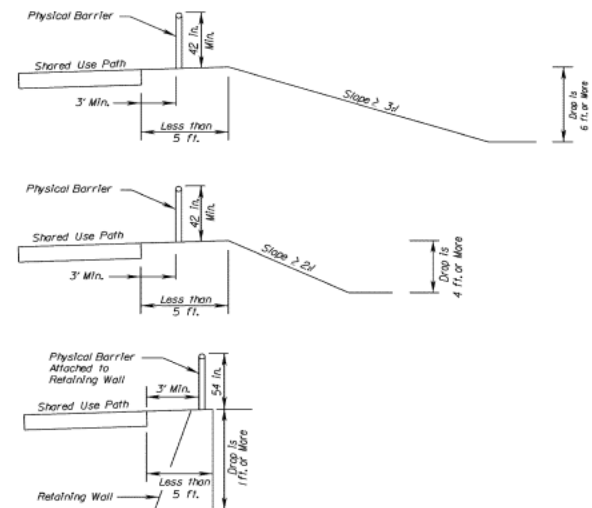


AASHTO APPROVED INTERSTATE BICYCLE ROUTES

VDOT provides signing along the designated AASHTO approved Interstate Bicycle Routes. Figure A-5-10 shows the corridors for Interstate Bicycle Routes 1 and 76 and the counties the routes pass through. The individual county maps provide detailed location information. County maps are to be checked by the plan designer to determine if their project is on a designated Interstate Bicycle Route. All proposed projects involving major construction or redevelopment along designated Interstate Bicycle Routes are to provide the necessary design features to facilitate bicycle travel in accordance with the parameters established in these guidelines.

\* Rev. 7/16

When physical barrier, railing (HR-1, Type III) or chain link fence are required. See Road and Bridge Standards for details.

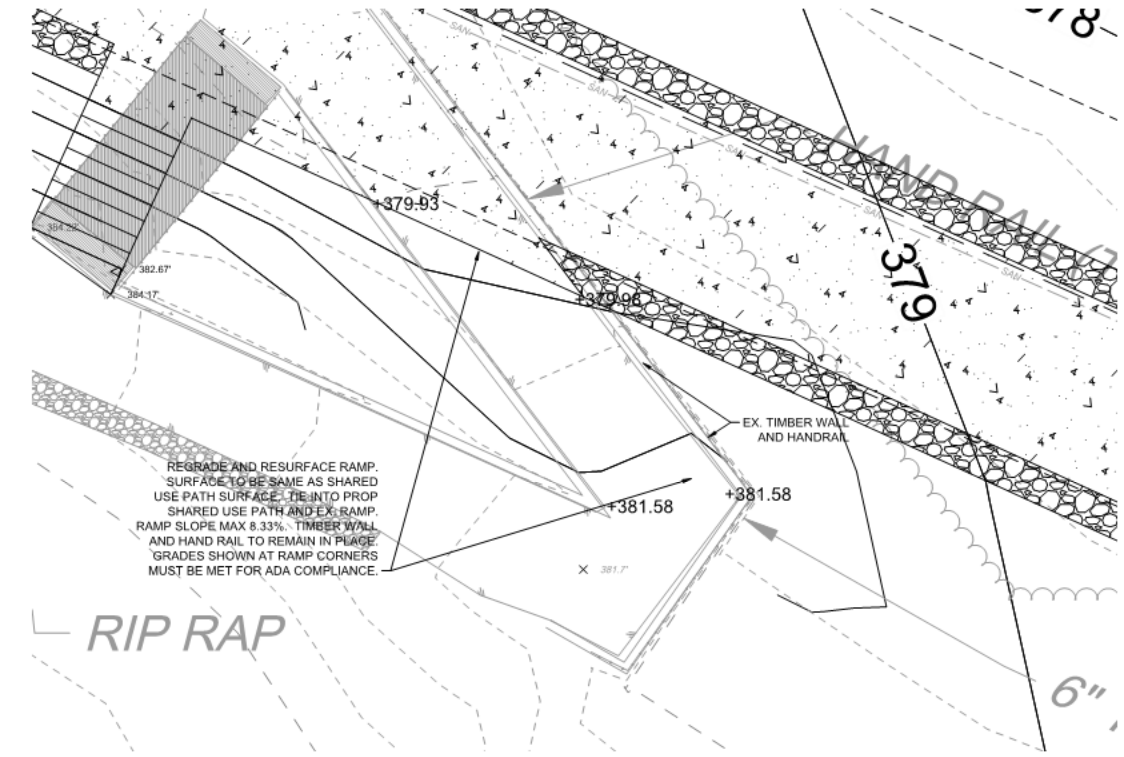
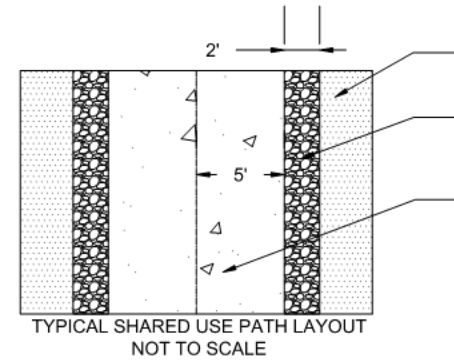


\* Rev. 1/11

GRASS SHOULDER

2' WIDE SHOULDER  
VDOT 21A MATERIAL

10' WIDE  
ASHPALT TRAIL



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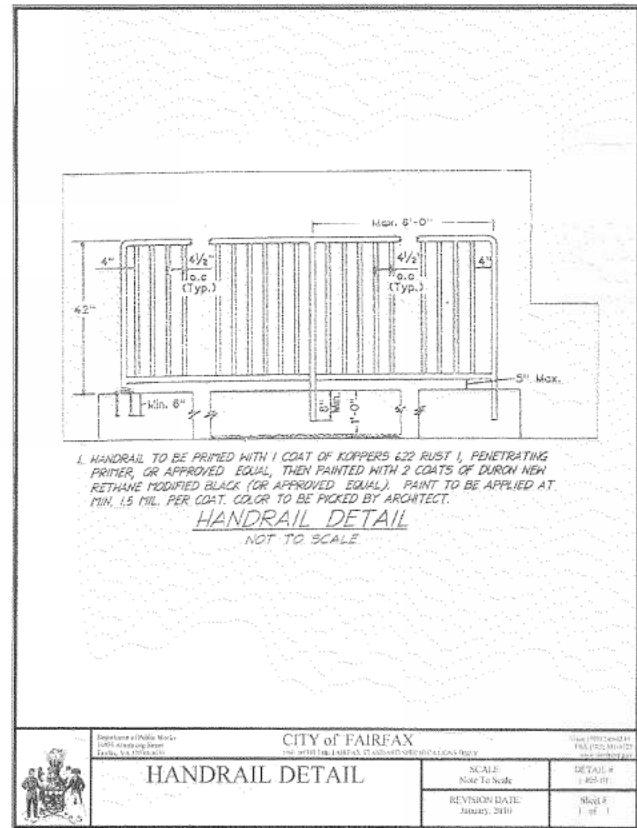
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JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA

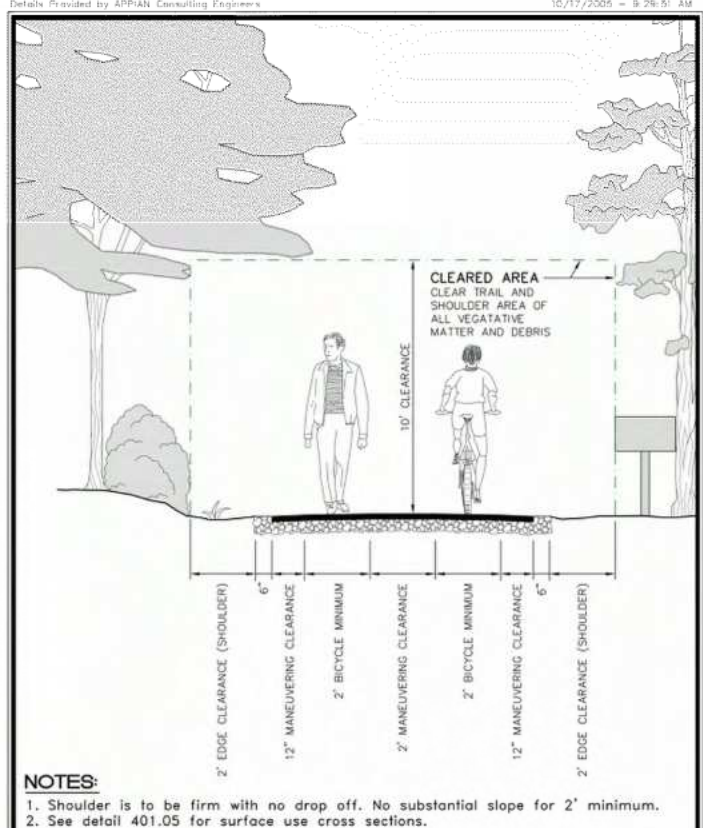
TRAIL NOTES AND DETAILS

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35061.030

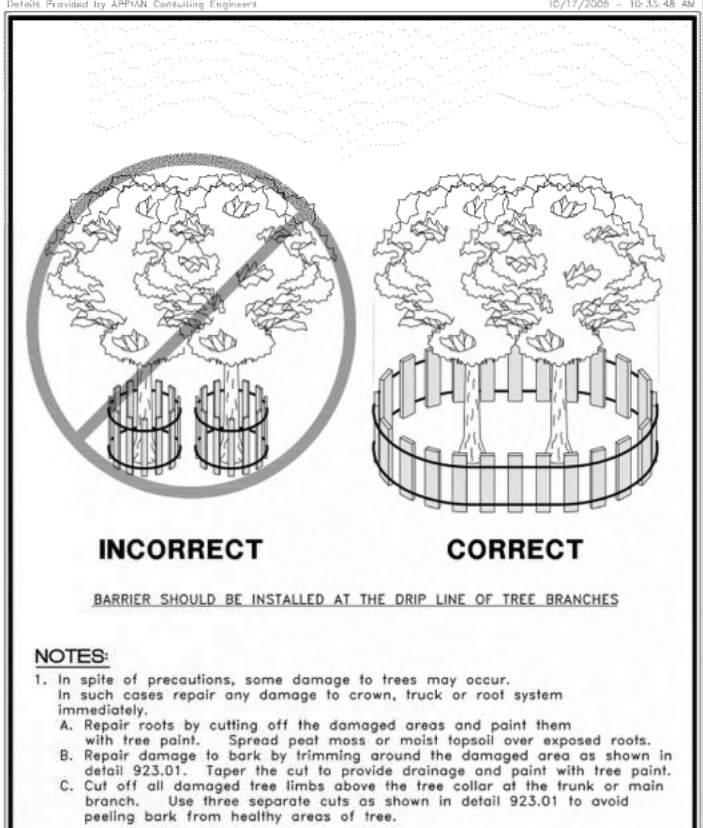
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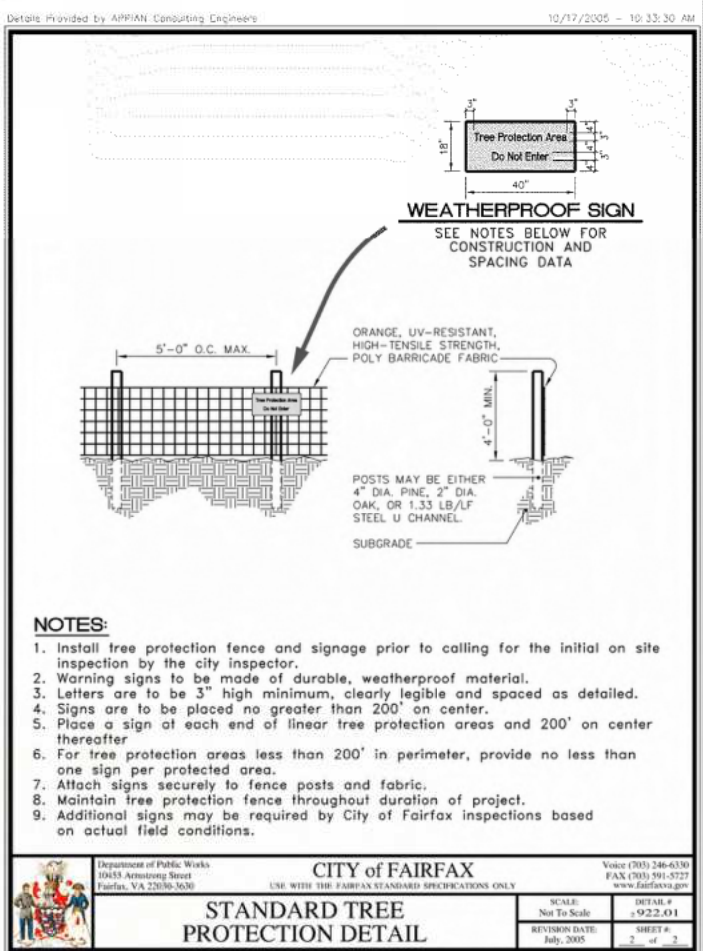
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		SCALE: Not To Scale		DETAIL #: 922.01	
		REVISION DATE: July, 2005		SHEET #: 1 of 1	



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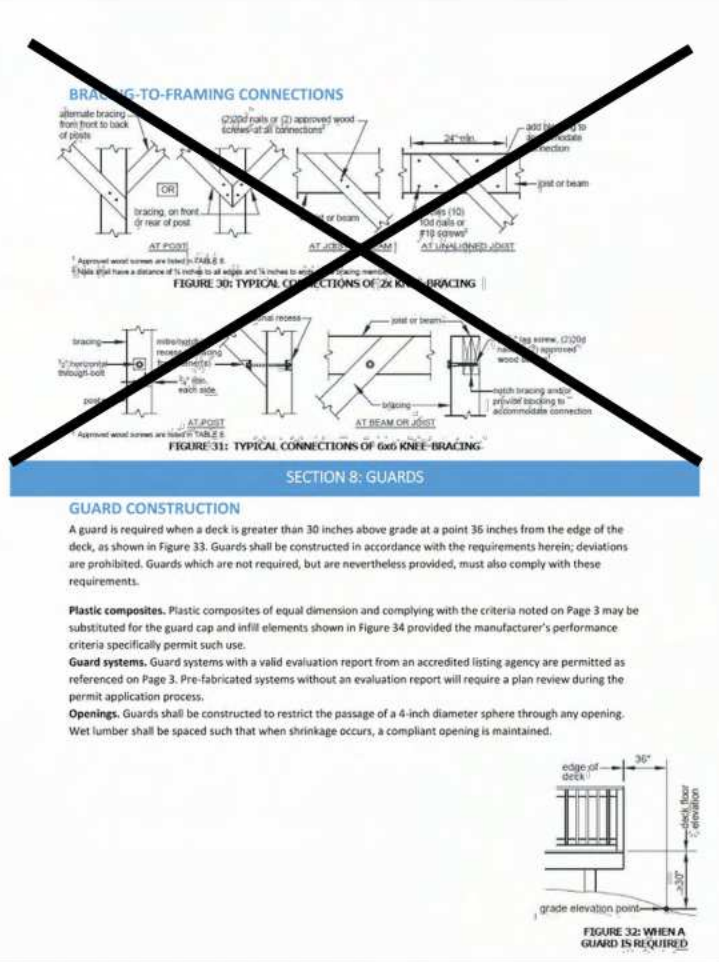


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		SCALE: Not To Scale		DETAIL #: 922.01	
		REVISION DATE: July, 2005		SHEET #: 2 of 2	

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**TIMMONS GROUP**  
 JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
 TRAIL NOTES AND DETAILS  
 JOB NO.: 35061.030  
 SHEET NO.: C5.5

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**SECTION 8: GUARDS**

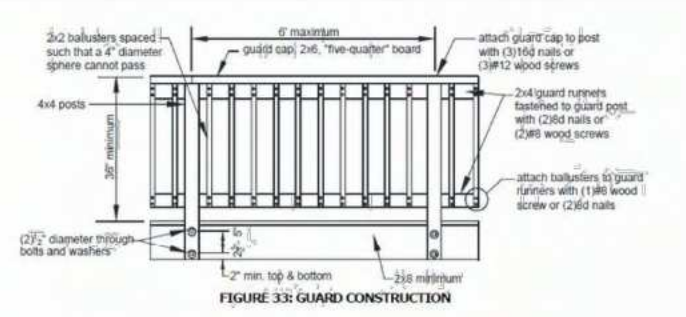
**GUARD CONSTRUCTION**

A guard is required when a deck is greater than 30 inches above grade at a point 36 inches from the edge of the deck, as shown in Figure 33. Guards shall be constructed in accordance with the requirements herein; deviations are prohibited. Guards which are not required, but are nevertheless provided, must also comply with these requirements.

**Plastic composites.** Plastic composites of equal dimension and complying with the criteria noted on Page 3 may be substituted for the guard cap and infill elements shown in Figure 34 provided the manufacturer's performance criteria specifically permit such use.

**Guard systems.** Guard systems with a valid evaluation report from an accredited listing agency are permitted as referenced on Page 3. Pre-fabricated systems without an evaluation report will require a plan review during the permit application process.

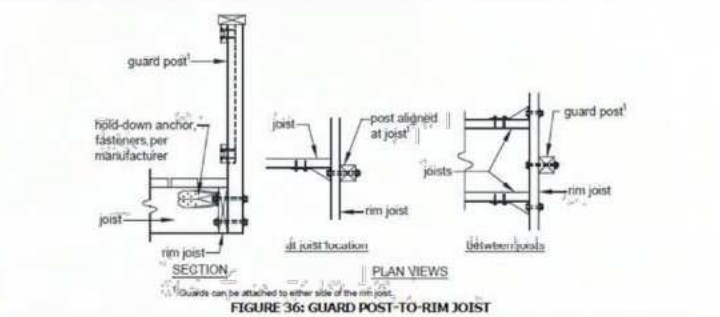
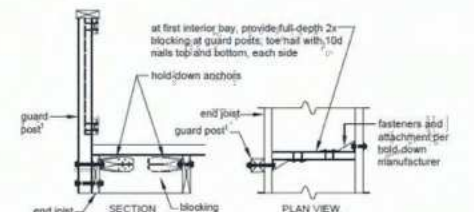
**Openings.** Guards shall be constructed to restrict the passage of a 4-inch diameter sphere through any opening. Wet lumber shall be spaced such that when shrinkage occurs, a compliant opening is maintained.



**GUARD POST CONNECTION**

Guard posts shall be attached to the deck structure in accordance with the requirements below in order to ensure resistance to imposed loads.

- Notching guard posts, as shown in Figure 35, is prohibited.
- Hold-down anchors, as shown in Figures 37 and 38, shall be used to attach the guard post to the end joist and rim joist, respectively.
- Hold-down anchors shall have a minimum capacity of 1,800 pounds.
- Guards may be attached to either side of the rim joist.



**SECTION 9: STAIRS**

**STAIR GEOMETRY**

Stairs shall be constructed with the dimensions listed below.

- The minimum width of a stairway is 36 inches.
- Stair geometry and opening limitations shall meet the requirements shown in Figure 38. Treads, risers and nosing dimensions shall not deviate at each step by more than 3/8 inches.

**STAIR LANDING**

- If the total vertical height of a stairway exceeds 12 feet, then an intermediate landing is required and must be constructed as a free-standing deck.
- Stair landings may be constructed with 4x4 posts with heights no greater than 8 feet.
- Landing widths shall be equal to the total width(s) of the stairway(s) served.

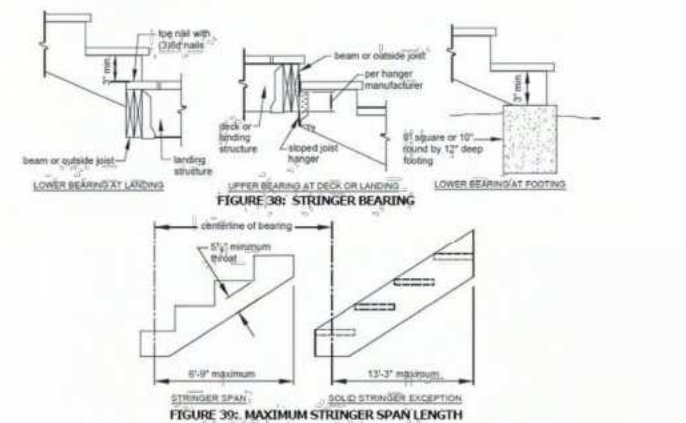
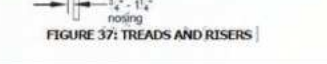
**STAIR CONSTRUCTION**

Stair stringers:

- Stringers shall be sawn or solid 2x12s complying with the tread and riser geometry requirements.
- Stringers shall be spaced at a maximum of 18 inches on center.
- Stringers shall bear on footings and attach to the deck or landing per Figure 39.
- Stringer span length is measured using the horizontally projected distance between the bearing at each end and shall not exceed the dimensions shown in Figure 40.

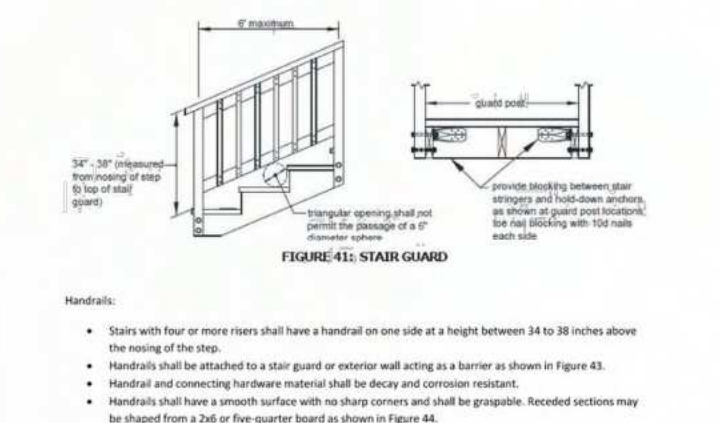
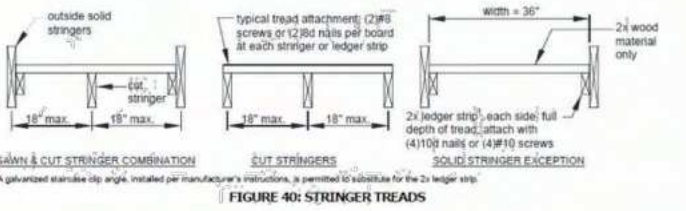
**SOLID STRINGER EXCEPTION:** Solid stringers of stairways with a width equal to 36 inches shall be permitted to have a span as shown in Figure 40.

- Throat size of cut stringers shall not exceed the value shown in Figure 40.



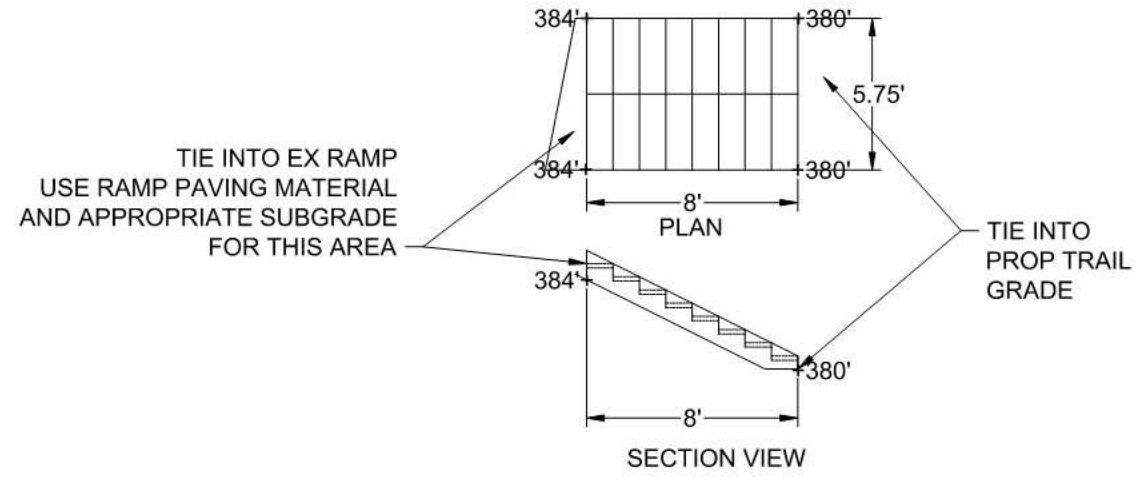
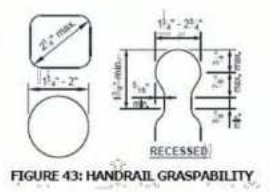
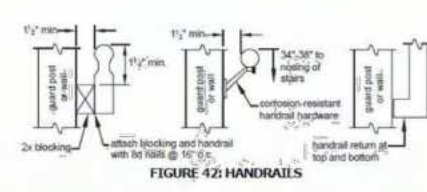
**Tread and riser material:**

- Tread material shall be equivalent to the decking specified on Page 4 and attached in accordance with Figure 41. The span of plastic composites shall be per manufacturer and in some cases may be less than 18 inches specified in Figure 41.
- Stairs constructed using the solid stringer exception shall have treads constructed of 2x wood material only; see Figure 41.
- Risers may be framed with 1x lumber minimum or equivalent plastic composite. Open risers are permitted provided the opening does not allow the passage of a 4-inch diameter sphere.



**Handrails:**

- Stairs with four or more risers shall have a handrail on one side at a height between 34 to 38 inches above the nosing of the step.
- Handrails shall be attached to a stair guard or exterior wall acting as a barrier as shown in Figure 43.
- Handrail and connecting hardware material shall be decay and corrosion resistant.
- Handrails shall have a smooth surface with no sharp corners and shall be graspable. Recessed sections may be shaped from a 2x6 or five-quarter board as shown in Figure 44.
- Handrails shall run continuously from a point directly over the lowest riser to a point directly over the highest riser and shall return to the guard or wall at each end.
- Handrails may be interrupted by guard posts at a turn in the stair only.
- Handrails installed in lieu of window safety glazing, as required on Page 5, shall be supported at appropriate intervals to ensure that when a 50-pound load is applied, the rail does not deflect into the glass.



**STAIR DETAILS**

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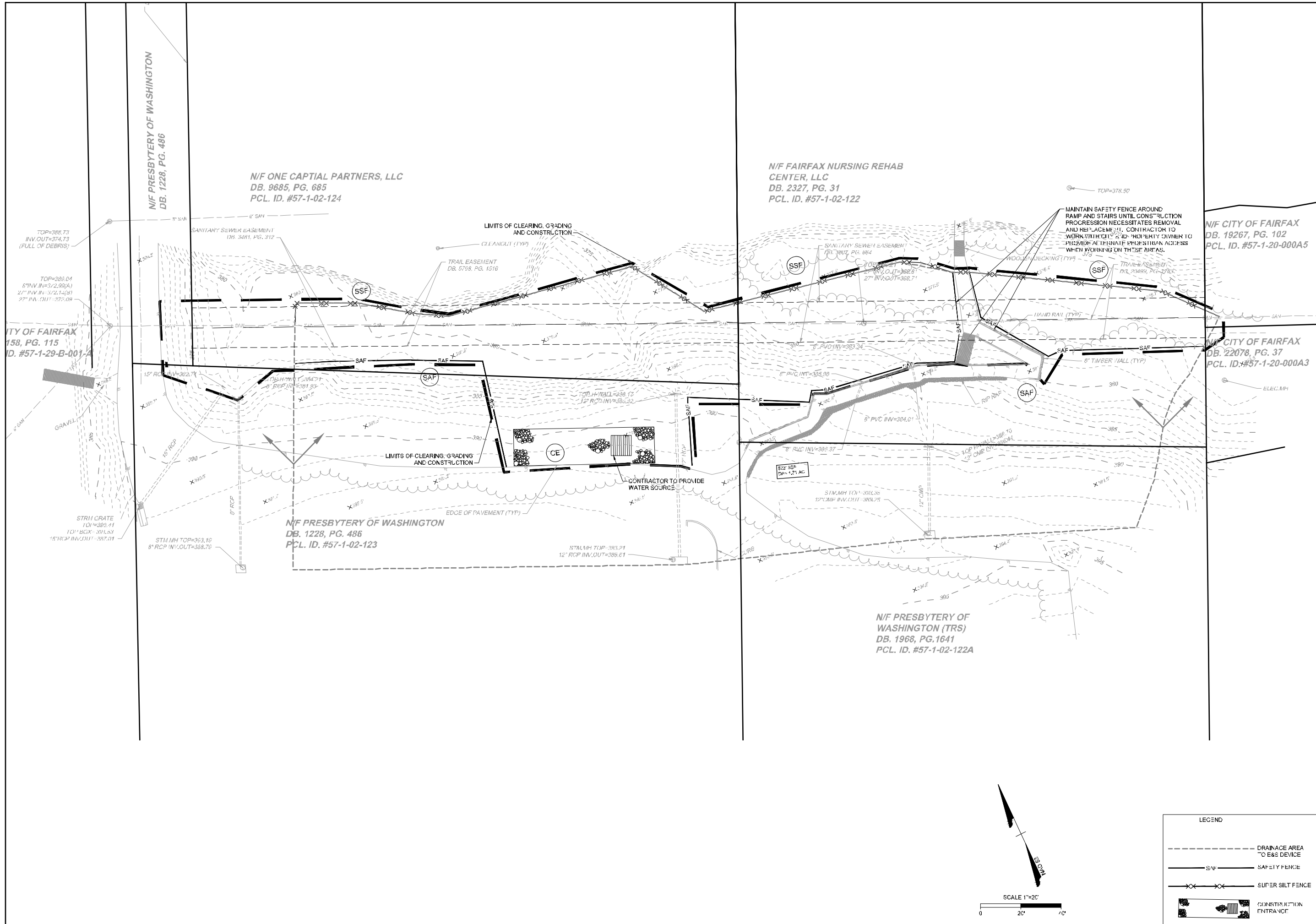
**TIMMONS GROUP**

JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA

STAIR DETAILS

JOB NO. 35061.030  
SHEET NO. C5.6

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**JUDICIAL DRIVE TRAIL CONNECTION**  
CITY OF FAIRFAX, VIRGINIA

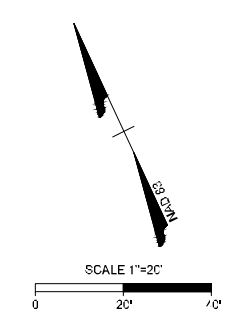
**EROSION AND SEDIMENT CONTROL PHASE I**

JOB NO.: 35061.030  
SHEET NO.: C6.0

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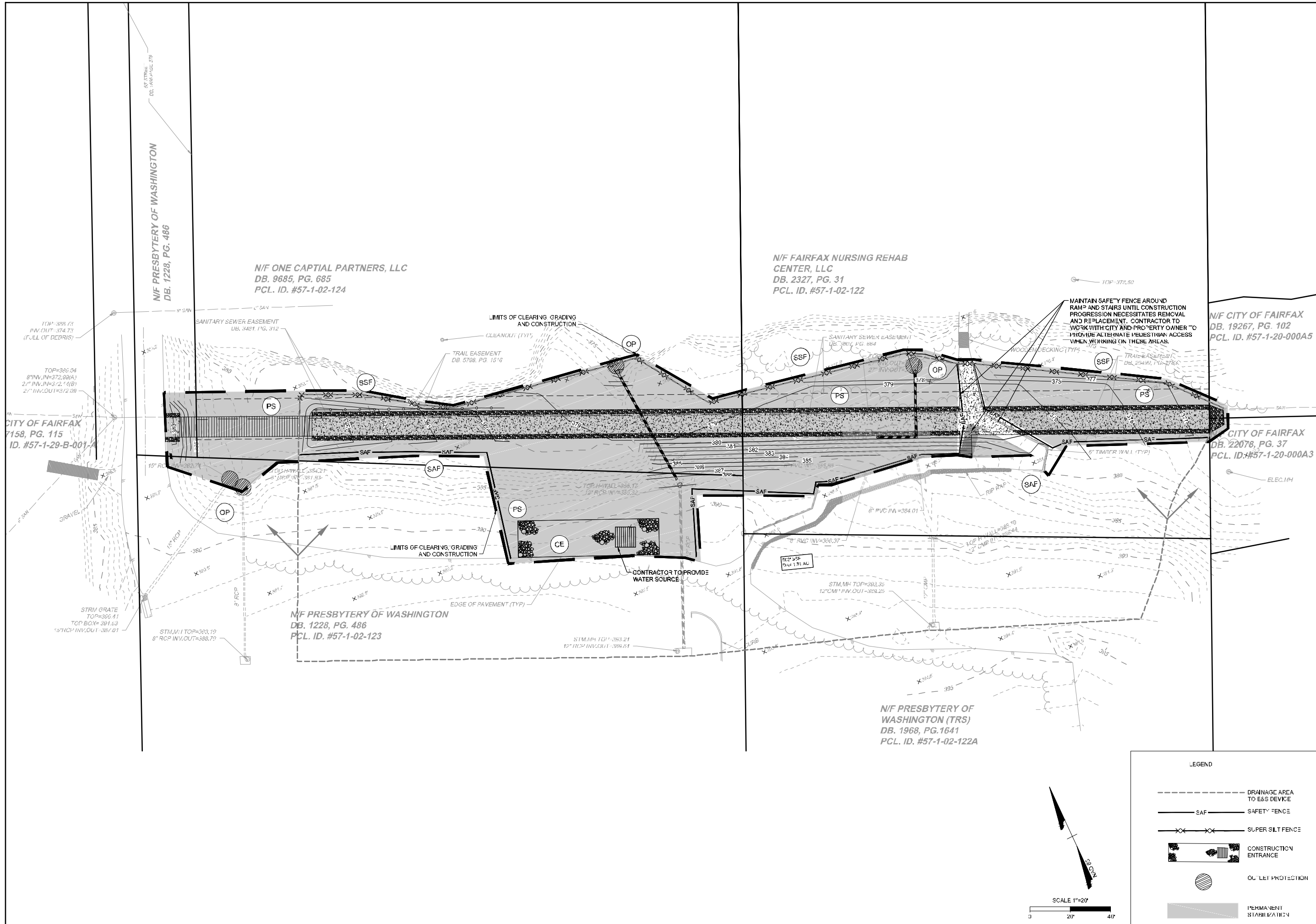
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LEGEND	
---	DRAINAGE AREA TO E&S DEVICE
— SAF —	SAFETY FENCE
XX XX	SUPER SILT FENCE
[Symbol]	CONSTRUCTION ENTRANCE

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**JUDICIAL DRIVE TRAIL CONNECTION**  
 CITY OF FAIRFAX, VIRGINIA

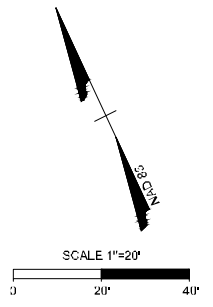
**EROSION AND SEDIMENT CONTROL PLAN - PHASE II**

JOB NO.: 35061.030  
 SHEET NO.: C6.1



**LEGEND**

- DRAINAGE AREA TO E&S DEVICE
- SAF SAFETY FENCE
- SUPER SILT FENCE
- CONSTRUCTION ENTRANCE
- OUL LET PROTECTION
- PERMANENT STABILIZATION



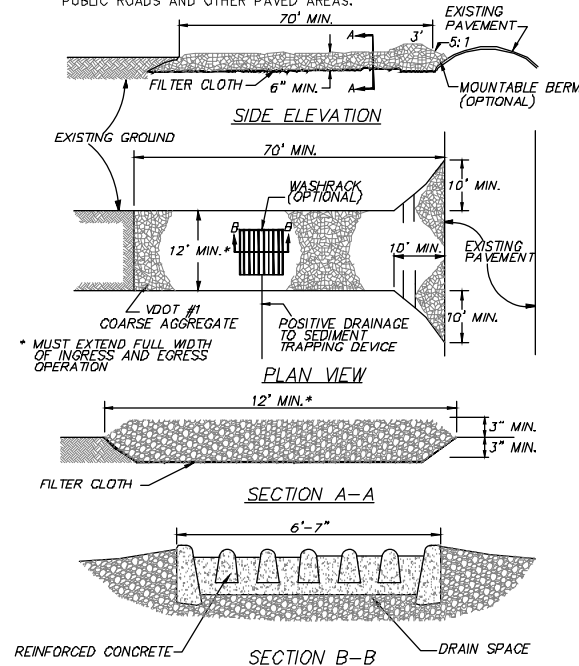
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# CE TEMPORARY STONE CONSTRUCTION ENTRANCE

VIRGINIA EROSION & SEDIMENT CONTROL 3.02:

A STONE PAD, LOCATED AT POINTS OF VEHICULAR INGRESS AND EGRESS ON A CONSTRUCTION SITE, TO REDUCE THE SOIL TRANSPORTED ONTO PUBLIC ROADS AND OTHER PAVED AREAS.



# CE STONE CONSTRUCTION ENTRANCE (3.02-1) NO SCALE

## CONSTRUCTION SPECIFICATIONS:

THE AREA OF THE ENTRANCE MUST BE EXCAVATED A MINIMUM OF 3 INCHES AND MUST BE CLEARED OF ALL VEGETATION, ROOTS, AND OTHER OBJECTIONABLE MATERIAL. THE FILTER FABRIC UNDERLINER WILL THEN BE PLACED THE FULL WIDTH AND LENGTH OF THE ENTRANCE.

FOLLOWING THE INSTALLATION OF THE FILTER CLOTH, THE STONE SHALL BE PLACED TO THE SPECIFIED DIMENSIONS. IF WASH RACKS ARE USED, THEY SHOULD BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHOULD BE CONSTRUCTED ACCORDING TO SPECIFICATIONS. CONVEYANCE OF SURFACE WATER UNDER ENTRANCE, THROUGH CULVERTS, SHALL BE PROVIDED AS REQUIRED. IF SUCH CONVEYANCE IS IMPOSSIBLE, THE CONSTRUCTION OF A "MOUNTABLE" BERM WITH 5:1 SLOPES SHALL BE PERMITTED.

THE FILTER CLOTH UTILIZED SHALL BE A WOVEN OR NONWOVEN FABRIC CONSISTING ONLY OF CONTINUOUS CHAIN POLYMERIC FILAMENTS OR YARNS OF POLYESTER. THE FABRIC SHALL BE INERT TO COMMONLY ENCOUNTERED CHEMICALS AND HYDROCARBONS, BE MILDEW AND ROT RESISTANT, AND CONFORM TO THE PHYSICAL PROPERTIES NOTED IN TABLE 3.02-A.

## MAINTENANCE:

THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR THE WASHING AND REWORKING OF EXISTING STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY STRUCTURES USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY. THE USE OF WATER TRUCKS TO REMOVE MATERIALS DROPPED, WASHED, OR TRACKED ONTO ROADWAYS WILL NOT BE PERMITTED UNDER ANY CIRCUMSTANCES.

# TEMPORARY SEEDING

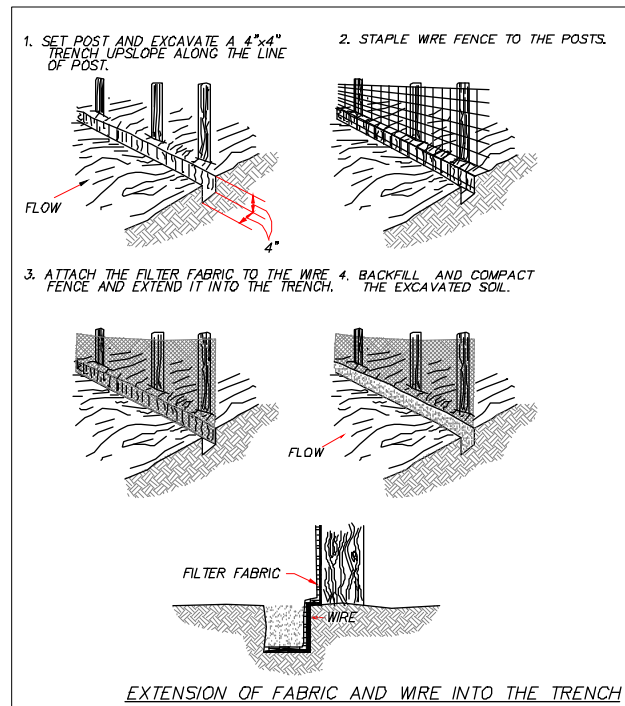
TABLE 3.01-B ACCEPTABLE TEMPORARY SEEDING PLANT MATERIALS "QUICK REFERENCE FOR ALL REGIONS"		
Planting Dates	Species	Rate (lbs./acre)
Sept. 1 - Feb. 15	50/50 Mix of Annual Ryegrass ( <i>Lolium multi-florum</i> ) & Cereal (Winter) Rye ( <i>Secale cereale</i> )	50 - 100
Feb. 16 - Apr. 30	Annual Ryegrass ( <i>Lolium multi-florum</i> )	60 - 100
May 1 - Aug 31	German Millet ( <i>Setaria italica</i> )	50

Source: Va. DSWC

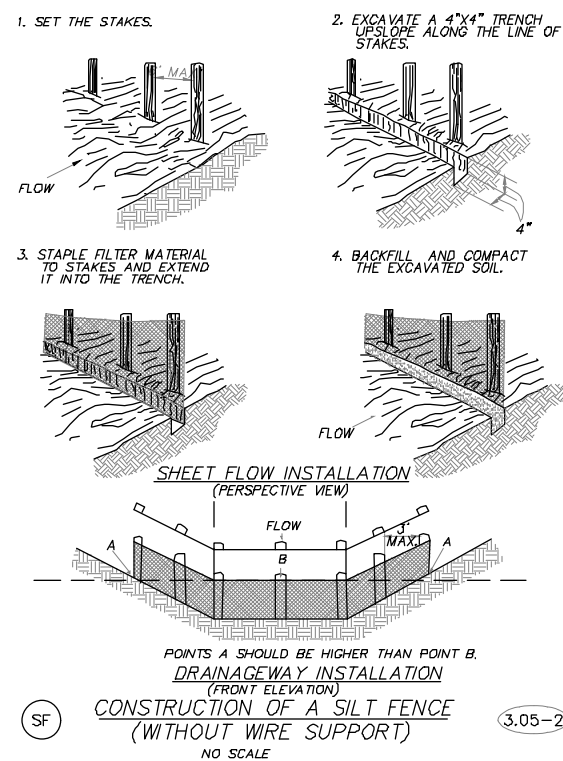
# SF SILT FENCE/SUPER SILT FENCE

VIRGINIA EROSION & SEDIMENT CONTROL 3.05:

A TEMPORARY SEDIMENT BARRIER CONSTRUCTED OF POSTS, FILTER FABRIC AND, IN SOME CASES, A WIRE SUPPORT FENCE, PLACED ACROSS OR AT THE TOE OF A SLOPE OR IN A MINOR DRAINAGE WAY TO INTERCEPT AND DETAIN SEDIMENT AND DECREASE FLOW VELOCITIES FROM DRAINAGE AREAS OF LIMITED SIZE; APPLICABLE WHERE SHEET AND RILL EROSION OR SMALL CONCENTRATED FLOWS MAY BE A PROBLEM. MAXIMUM EFFECTIVE LIFE OF 6 MONTHS.



# SF CONSTRUCTION OF A SILT FENCE (WITH WIRE SUPPORT) (3.05-1) NO SCALE



# SF CONSTRUCTION OF A SILT FENCE (WITHOUT WIRE SUPPORT) (3.05-2) NO SCALE

# SILT FENCE/SUPER SILT FENCE (continued)

## CONSTRUCTION SPECIFICATIONS:

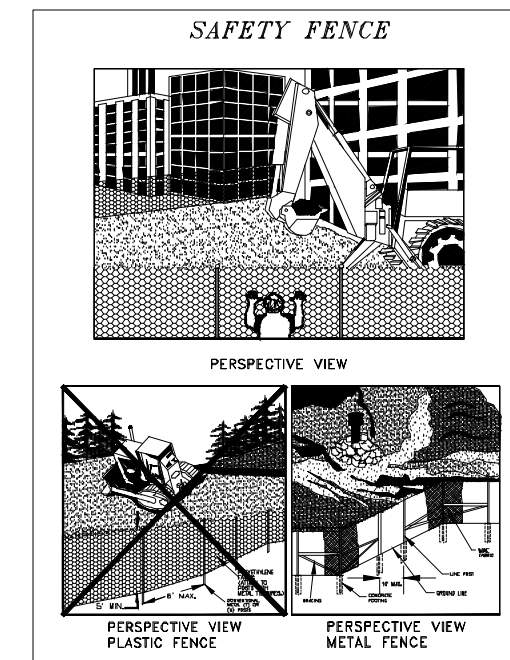
1. SYNTHETIC FILTER FABRIC SHALL BE A PERVIOUS SHEET OF PROPYLENE, NYLON, POLYESTER OR ETHYLENE YARN AND SHALL BE CERTIFIED BY THE MANUFACTURER OR SUPPLIER AS CONFORMING TO THE REQUIREMENTS NOTED IN TABLE 3.05-B.
2. SYNTHETIC FILTER FABRIC SHALL CONTAIN ULTRAVIOLET RAY INHIBITORS AND STABILIZERS TO PROVIDE A MINIMUM OF SIX MONTHS OF EXPECTED USABLE CONSTRUCTION LIFE AT A TEMPERATURE RANGE OF 0° F TO 120° F.
3. IF WOODEN STAKES ARE UTILIZED FOR SILT FENCE CONSTRUCTION, THEY MUST HAVE A DIAMETER OF 2 INCHES WHEN OAK IS USED AND 4 INCHES WHEN PINE IS USED. WOODEN STAKES MUST HAVE A MINIMUM LENGTH OF 5 FEET.
4. IF STEEL POSTS (STANDARD "U" OR "T" SECTION) ARE UTILIZED FOR SILT FENCE CONSTRUCTION, THEY MUST HAVE A MINIMUM WEIGHT OF 1.33 POUNDS PER LINEAR FOOT AND SHALL HAVE A MINIMUM LENGTH OF 5 FEET.
5. WIRE FENCE REINFORCEMENT FOR SILT FENCES USING STANDARD-STRENGTH FILTER CLOTH SHALL BE A MINIMUM OF 14 GAUGE WIRE WITH A MAXIMUM MESH SPACING OF 6 INCHES.

1. SILT FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
2. CLOSE ATTENTION SHALL BE PAID TO THE REPAIR OF DAMAGED SILT FENCE RESULTING FROM END RUNS AND UNDERCUTTING.
3. SHOULD THE FABRIC ON A SILT FENCE DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL BE NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
4. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY ONE-HALF THE HEIGHT OF THE BARRIER.
5. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEEDED.

# SAF SAFETY FENCE

VIRGINIA EROSION & SEDIMENT CONTROL 3.01:

A PROTECTIVE BARRIER INSTALLED TO PROHIBIT UNDESIRABLE USE OF AN EROSION CONTROL MEASURE.



SOURCE: CONWED PLASTICS VDOT ROAD AND BRIDGE STANDARDS VA. DSWC PLATE 3.01-1

# EROSION CONTROL SEQUENCE PHASE I

1. A PRE-CONSTRUCTION CONFERENCE IS MANDATORY BEFORE ANY WORK IS DONE AT THE SITE. AFTER RECEIPT OF THE LAND DISTURBING PERMIT ARRANGE A MEETING WITH OWNER, ENGINEER AND CITY OF FAIRFAX INSPECTOR. 72 HOURS NOTICE IS REQUIRED, ONE WEEK IS PREFERRED.
2. PERIMETER SEDIMENT TRAPPING MEASURES MUST BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.
3. INSTALL NECESSARY PERIMETER E&S CONTROLS TO PERFORM GRADING REQUIRED TO CONSTRUCT ACCESS ONTO SITE.
4. INSTALL CONSTRUCTION ENTRANCE WITH WASH RACK. CONTRACTOR TO SUPPLY WATER SOURCE FOR WASH RACK.
5. CLEAR AND GRUB IN ORDER TO INSTALL THE PHASE I E&S CONTROLS (SF AND ALL OTHER PERIMETER CONTROLS).
6. CLEAR AND STRIP SITE. TEMPORARILY STOCK PILE TOPSOIL (IF ANY) ON SITE.
7. ONCE THE ES CONTROLS ARE IN PLACE, CONTRACTOR MAY COMMENCE PHASE II.

# EROSION CONTROL SEQUENCE PHASE II

1. PHASE I MEASURES SHALL BE MAINTAINED OR REPLACED AS AREAS ARE GRADED AND STABILIZED. PERMISSION TO REMOVE ANY E&S CONTROLS SHALL BE OBTAINED BY THE CONTRACTOR FROM THE CITY OF FAIRFAX E&S INSPECTOR.
2. COMPLETE SITE WORK CONSTRUCTION, INCLUDING GRADING, STORM SEWER AND UNDERDRAINS, CULVERTS, AND MULTI-USE TRAIL. TEMPORARY SEED ALL DISTURBED AREAS WHICH WILL NOT BE BROUGHT TO FINAL GRADE FOR A PERIOD OF MORE THAN 14 DAYS.
3. COMPLETE FINE GRADING, PLACEMENT OF SLOPE EROSION CONTROL BLANKET, AND PERMANENT SEEDING.
4. CONSTRUCT FINAL PAVEMENT AND TRAIL CONSTRUCTION. INSTALL SIGNAGE.
5. STABILIZE ANY REMAINING PROBLEM AREAS ON SITE.
6. REMOVE CONTRACTOR STAGING AREAS AND RE-ESTABLISH STAND OF GRASS.
7. DO NOT REMOVE EROSION CONTROL MEASURES UNTIL ALL CONTRIBUTING AREAS HAVE BEEN PERMANENTLY STABILIZED.
8. THE CITY OF FAIRFAX E&S INSPECTOR HAS THE AUTHORITY TO ADD OR DELETE E&S CONTROL DEVICES IN THE FIELD AS THE SITE CONDITIONS WARRANT. NO E&S CONTROL DEVICE SHALL BE REMOVED WITHOUT PRIOR APPROVAL OF THE CITY OF FAIRFAX E&S INSPECTOR.

## CONSTRUCTION SPECIFICATIONS:

1. SAFETY FENCES MUST BE INSTALLED PRIOR TO THE E & S MEASURE BECOMING ACCESSIBLE.
2. THE POLYETHYLENE WEB OF THE PLASTIC SAFETY FENCE SHALL BE SECURED TO A CONVENTIONAL METAL "T" OR "U" POST DRIVEN INTO THE GROUND TO A MINIMUM DEPTH OF 18 INCHES. POSTS SHOULD BE SPACED AT 6-FOOT CENTERS. SEE "PERSPECTIVE" VIEW IN PLATE 3.01-1.
3. THE METAL SAFETY FENCE SHALL BE INSTALLED AS PER THE FOLLOWING PROCEDURE:
  - A. LINE POSTS SHALL BE PLACED AT INTERVALS OF 10 FEET MEASURED FROM CENTER TO CENTER OF ADJACENT POSTS. IN DETERMINING THE POST SPACING, MEASUREMENT WILL BE MADE PARALLEL WITH THE GROUND SURFACE. SEE "PERSPECTIVE" VIEW IN PLATE 3.01-1.
  - B. POSTS WILL BE SET IN CONCRETE BLOCKS.
  - C. ALL CORNER POSTS, END POSTS, GATE POSTS, AND PULL POSTS SHALL BE EMBEDDED, BRACED, AND TRUSSED AS SHOWN IN THE "STANDARD FENCE - CHAIN LINK" DETAIL FOUND IN THE LATEST VERSION OF THE VDOT ROAD AND BRIDGE STANDARDS.
  - D. FENCING FABRIC SHALL NOT BE STRETCHED UNTIL AT LEAST 4 DAYS AFTER THE POSTS ARE GROUTED INTO WALLS OR 14 DAYS AFTER THE POSTS ARE SET INTO CONCRETE.
  - E. THE FABRIC SHALL BE STRETCHED TAUT AND SECURELY FASTENED, BY MEANS OF THE CLIPS, TO THE POSTS AT INTERVALS NOT EXCEEDING 15 INCHES AND TO THE TOP RAILS OR TENSION WIRES AT INTERVALS NOT EXCEEDING 2 FEET. CARE SHALL BE TAKEN TO EQUALIZE THE TENSION ON EACH SIDE OF EACH POST.
    1. APPLICABLE WARNING SIGNS NOTING HAZARDOUS CONDITIONS MUST BE INSTALLED IMMEDIATELY UPON INSTALLATION OF SAFETY FENCE.

## MAINTENANCE:

1. SAFETY FENCE SHALL BE CHECKED REGULARLY FOR WEATHER-RELATED OR OTHER DAMAGE. ANY NECESSARY REPAIRS MUST BE MADE IMMEDIATELY.
2. CARE SHOULD BE TAKEN TO SECURE ALL ACCESS POINTS (GATES) AT THE END OF EACH WORKING DAY. ALL LOCKING DEVICES MUST BE REPAIRED OR REPLACED AS NECESSARY.

**TIMMONS GROUP**  
 JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
 EROSION AND SEDIMENT CONTROL NOTES AND DETAILS  
 JOB NO.: 35061.030  
 SHEET NO.: C6.3

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**VEGETATED FILTER STRIP NOTES**

**VEGETATED FILTER STRIP (DESIGN SPECIFICATION No. 2)**

VEGETATED FILTER STRIPS ARE BEST SUITED TO TREAT RUNOFF FROM SMALL SEGMENTS OF IMPERVIOUS COVER (USUALLY LESS THAN 5,000 SQ. FT. ) ADJACENT TO ROAD SHOULDERS, SMALL PARKING LOTS AND ROOFTOPS. VEGETATED FILTER STRIPS MAY ALSO BE USED AS PRETREATMENT FOR ANOTHER STORMWATER PRACTICE SUCH AS A DRY SWALE, BIORETENTION, OR INFILTRATION AREAS. IF SUFFICIENT PERVIOUS AREA IS AVAILABLE AT THE SITE, LARGER AREAS OF IMPERVIOUS COVER CAN BE TREATED BY VEGETATED FILTER STRIPS, USING AN ENGINEERED LEVEL SPREADER TO RECREATE SHEET FLOW. VEGETATED FILTER STRIPS ARE ALSO WELL SUITED TO TREAT RUNOFF FROM TURF-INTENSIVE LAND USES, SUCH AS THE MANAGED TURF AREAS OF SPORTS FIELDS, GOLF COURSES, AND PARKLAND.

CONSERVED OPEN SPACE AND VEGETATED FILTER STRIPS CAN BE USED IN A VARIETY OF SITUATIONS; HOWEVER THERE ARE SEVERAL CONSTRAINTS TO THEIR USE:

- **FILTER SLOPES AND WIDTHS.** MAXIMUM SLOPES FOR CONSERVED OPEN SPACE AND VEGETATED FILTER STRIPS ARE 6% AND 8% RESPECTIVELY, IN ORDER TO MAINTAIN SHEET FLOW THROUGH THE PRACTICE. IN ADDITION, THE OVERALL CONTRIBUTING DRAINAGE AREA MUST LIKEWISE BE RELATIVELY FLAT TO ENSURE SHEET FLOW DRAINING INTO THE FILTER, WHERE THIS IS NOT POSSIBLE, ALTERNATIVE MEASURES, SUCH AS AN ENGINEERED LEVEL SPREADER, CAN BE USED. MINIMUM WIDTHS (FLOW PATH) FOR CONSERVED OPEN SPACE AND VEGETATED FILTER STRIPS ARE DEPENDENT ON SLOPE, AS SPECIFIED IN **TABLE 2.2** ABOVE.
- **SOILS.** VEGETATED FILTER STRIPS ARE APPROPRIATE FOR ALL SOIL TYPES, EXCEPT FILL SOILS. THE RUNOFF REDUCTION RATE, HOWEVER, IS DEPENDENT ON THE UNDERLYING HYDROLOGIC SOIL GROUPS (SEE **TABLE 2.1** ABOVE) AND WHETHER SOILS RECEIVE COMPOST AMENDMENTS.
- **CONTRIBUTING FLOW PATH TO FILTER.** VEGETATED FILTER STRIPS ARE USED TO TREAT VERY SMALL DRAINAGE AREAS OF A FEW ACRES OR LESS. THE LIMITING DESIGN FACTOR IS THE LENGTH OF FLOW DIRECTED TO THE FILTER. AS A RULE, FLOW TENDS TO CONCENTRATE AFTER 75 FEET OF FLOW LENGTH FOR IMPERVIOUS SURFACES, AND 150 FEET FOR PERVIOUS SURFACES (CLAYTOR, 1996). WHEN FLOW CONCENTRATES, IT MOVES TOO RAPIDLY TO BE EFFECTIVELY TREATED BY A VEGETATED FILTER STRIP, UNLESS AN ENGINEERED LEVEL SPREADER IS USED. WHEN THE EXISTING FLOW AT A SITE IS CONCENTRATED, A VEGETATED SWALE SHOULD BE USED INSTEAD OF A VEGETATED FILTER STRIP (LANTIN AND BARRETT, 2005).
- **HOTSPOT LAND USES.** VEGETATED FILTER STRIPS SHOULD NOT RECEIVE HOTSPOT RUNOFF, SINCE THE INFILTRATED RUNOFF COULD CAUSE GROUNDWATER CONTAMINATION.
- **TURF-INTENSIVE LAND USES.** BOTH CONSERVED OPEN SPACE AND VEGETATED FILTER STRIPS ARE APPROPRIATE TO TREAT MANAGED TURF AND THE ACTIVELY-USED AREAS OF SPORTS FIELDS, GOLF COURSES, PARKLAND, AND OTHER TURF-INTENSIVE LAND USES.
- **PROXIMITY OF UNDERGROUND UTILITIES.** UNDERGROUND PIPES AND CONDUITS THAT CROSS THE VEGETATED FILTER STRIP ARE ACCEPTABLE.

**COMPOST SOIL AMENDMENTS**

COMPOST SOIL AMENDMENTS WILL ENHANCE THE RUNOFF REDUCTION CAPABILITY OF A VEGETATED FILTER STRIP WHEN LOCATED ON HYDROLOGIC SOIL GROUPS B, C, AND D, SUBJECT TO THE FOLLOWING DESIGN REQUIREMENTS:

- THE COMPOST AMENDMENTS SHOULD EXTEND OVER THE FULL LENGTH AND WIDTH OF THE FILTER STRIP.
- THE AMOUNT OF APPROVED COMPOST MATERIAL AND THE DEPTH TO WHICH IT MUST BE INCORPORATED IS OUTLINED IN STORMWATER DESIGN SPECIFICATION NO. 4.
- THE AMENDED AREA WILL BE RAKED TO ACHIEVE THE MOST LEVEL SLOPE POSSIBLE WITHOUT USING HEAVY CONSTRUCTION EQUIPMENT, AND IT WILL BE STABILIZED RAPIDLY WITH PERENNIAL GRASS AND/OR HERBACEOUS SPECIES.
- IF SLOPES EXCEED 3%, A PROTECTIVE BIODEGRADABLE FABRIC OR MATTING (E.G., EC-2) SHOULD BE INSTALLED TO STABILIZE THE SITE PRIOR TO RUNOFF DISCHARGE.
- COMPOST AMENDMENTS SHOULD NOT BE INCORPORATED UNTIL THE GRAVEL DIAPHRAGM AND/OR ENGINEERED LEVEL SPREADER ARE INSTALLED (SEE **SECTION 6.3**).
- THE LOCAL PLAN APPROVAL AUTHORITY MAY WAIVE THE REQUIREMENT FOR COMPOST AMENDMENTS ON HSG-B SOILS IN ORDER TO RECEIVE CREDIT AS A FILTER STRIP IF (1) THE DESIGNER CAN PROVIDE VERIFICATION OF THE ADEQUACY OF THE ON-SITE SOIL TYPE, TEXTURE, AND PROFILE TO FUNCTION AS A FILTER STRIP, AND (2) THE AREA DESIGNATED FOR THE FILTER STRIP WILL NOT BE DISTURBED DURING CONSTRUCTION.

**PLANTING AND VEGETATION MANAGEMENT**

**VEGETATED FILTER STRIPS.** VEGETATED FILTER STRIPS SHOULD BE PLANTED AT SUCH A DENSITY TO ACHIEVE A 90% GRASS/HERBACEOUS COVER AFTER THE SECOND GROWING SEASON. FILTER STRIPS SHOULD BE SEEDED, NOT SODDED. SEEDING ESTABLISHES DEEPER ROOTS, AND SOD MAY HAVE MUCK SOIL THAT IS NOT CONDUCTIVE TO INFILTRATION (WISCONSIN DNR, 2007). THE FILTER STRIP VEGETATION MAY CONSIST OF TURF GRASSES, MEADOW GRASSES, OTHER HERBACEOUS PLANTS, SHRUBS, AND TREES, AS LONG AS THE PRIMARY GOAL OF AT LEAST 90% COVERAGE WITH GRASSES AND/OR OTHER HERBACEOUS PLANTS IS ACHIEVED. DESIGNERS SHOULD CHOOSE VEGETATION THAT STABILIZES THE SOIL AND IS SALT TOLERANT. VEGETATION AT THE TOE OF THE FILTER, WHERE TEMPORARY PONDING MAY OCCUR BEHIND THE PERMEABLE BERM, SHOULD BE ABLE TO WITHSTAND BOTH WET AND DRY PERIODS. THE PLANTING AREAS CAN BE DIVIDED INTO ZONES TO ACCOUNT FOR DIFFERENCES IN INUNDATION AND SLOPE.

**CONSTRUCTION SEQUENCE FOR VEGETATED FILTER STRIPS**

VEGETATED FILTER STRIPS CAN BE WITHIN THE LIMITS OF DISTURBANCE DURING CONSTRUCTION. THE FOLLOWING PROCEDURES SHOULD BE FOLLOWED DURING CONSTRUCTION:

- BEFORE SITE WORK BEGINS, VEGETATED FILTER STRIP BOUNDARIES SHOULD BE CLEARLY MARKED.
- ONLY VEHICULAR TRAFFIC USED FOR FILTER STRIP CONSTRUCTION SHOULD BE ALLOWED WITHIN 10 FEET OF THE FILTER STRIP BOUNDARY (CITY OF PORTLAND, 2004).
- IF EXISTING TOPSOIL IS STRIPPED DURING GRADING, IT SHALL BE STOCKPILED FOR LATER USE.
- CONSTRUCTION RUNOFF SHOULD BE DIRECTED AWAY FROM THE PROPOSED FILTER STRIP SITE, USING PERIMETER SILT FENCE, OR, PREFERABLY, A DIVERSION DIKE.
- CONSTRUCTION OF THE GRAVEL DIAPHRAGM OR ENGINEERED LEVEL SPREADER SHALL NOT COMMENCE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED AND PERIMETER E&S CONTROLS HAVE BEEN REMOVED AND CLEANED OUT.
- VEGETATED FILTER STRIPS REQUIRE LIGHT GRADING TO ACHIEVE DESIRED ELEVATIONS AND SLOPES. THIS SHOULD BE DONE WITH TRACKED VEHICLES TO PREVENT COMPACTION. TOPSOIL AND OR COMPOST AMENDMENTS SHOULD BE INCORPORATED EVENLY ACROSS THE FILTER STRIP AREA, STABILIZED WITH SEED, AND PROTECTED BY BIODEGRADABLE EROSION CONTROL MATTING OR BLANKETS.
- STORMWATER SHOULD NOT BE DIVERTED INTO THE FILTER STRIP UNTIL THE TURF COVER IS DENSE AND WELL ESTABLISHED.

**CONSTRUCTION INSPECTION**

CONSTRUCTION INSPECTION IS CRITICAL TO OBTAIN ADEQUATE SPOT ELEVATIONS, TO ENSURE THE GRAVEL DIAPHRAGM OR ELS IS COMPLETELY LEVEL, ON THE SAME CONTOUR, AND CONSTRUCTED TO THE CORRECT DESIGN ELEVATION. AS-BUILT SURVEYS SHOULD BE REQUIRED TO ENSURE COMPLIANCE WITH DESIGN STANDARDS. INSPECTORS SHOULD EVALUATE THE PERFORMANCE OF THE FILTER STRIP AFTER THE FIRST BIG STORM TO LOOK FOR EVIDENCE OF GULLIES, OUTFLANKING, UNDERCUTTING OR SPARSE VEGETATIVE COVER. SPOT REPAIRS SHOULD BE MADE, AS NEEDED.

**MAINTENANCE INSPECTIONS**

ANNUAL INSPECTIONS ARE USED TO TRIGGER MAINTENANCE OPERATIONS SUCH AS SEDIMENT REMOVAL, SPOT RE-VEGETATION AND LEVEL SPREADER REPAIR. IDEALLY, INSPECTIONS SHOULD BE CONDUCTED IN THE NON-GROWING SEASON WHEN IT EASIER TO SEE THE FLOW PATH. EXAMPLE MAINTENANCE INSPECTION CHECKLISTS FOR SHEET FLOW TO A FILTER STRIP OR CONSERVED OPEN SPACE AREAS CAN BE ACCESSED IN APPENDIX C OF CHAPTER 9 OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK.

MAINTENANCE CHECKLISTS) INSPECTORS SHOULD CHECK TO ENSURE THAT:

- FLOWS THROUGH THE FILTER STRIP DO NOT SHORT-CIRCUIT THE OVERFLOW CONTROL SECTION;
- DEBRIS AND SEDIMENT DOES NOT BUILD UP AT THE TOP OF THE FILTER STRIP;
- FOOT OR VEHICULAR TRAFFIC DOES NOT COMPROMISE THE GRAVEL DIAPHRAGM;
- SCOUR AND EROSION DO NOT OCCUR WITHIN THE FILTER STRIP;
- SEDIMENTS ARE CLEANED OUT OF LEVEL SPREADER FOREBAYS AND FLOW SPLITTERS; AND
- VEGETATIVE DENSITY EXCEEDS A 90% COVER IN THE BOUNDARY ZONE OR GRASS FILTER.

**ONGOING MAINTENANCE**

ONCE ESTABLISHED, VEGETATED FILTER STRIPS HAVE MINIMAL MAINTENANCE NEEDS OUTSIDE OF THE SPRING CLEAN UP, REGULAR MOWING, REPAIR OF CHECK DAMS AND OTHER MEASURES TO MAINTAIN THE HYDRAULIC EFFICIENCY OF THE STRIP AND A DENSE, HEALTHY GRASS COVER. VEGETATED FILTER STRIPS THAT CONSIST OF GRASS/TURF COVER SHOULD BE MOWED AT LEAST TWICE A YEAR TO PREVENT WOODY GROWTH.

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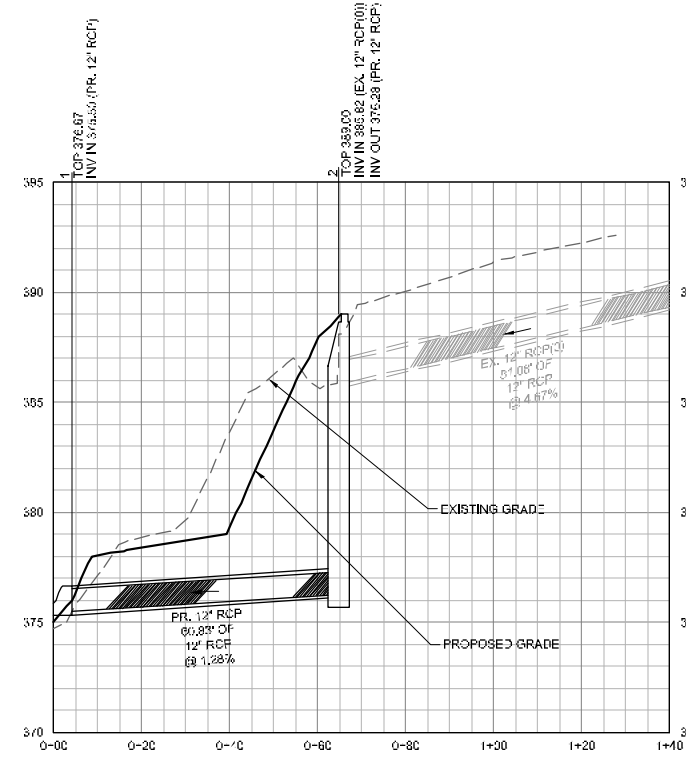
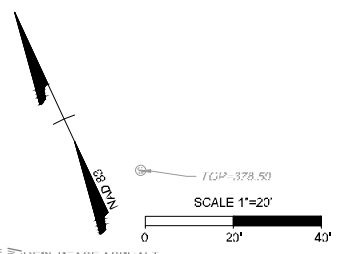
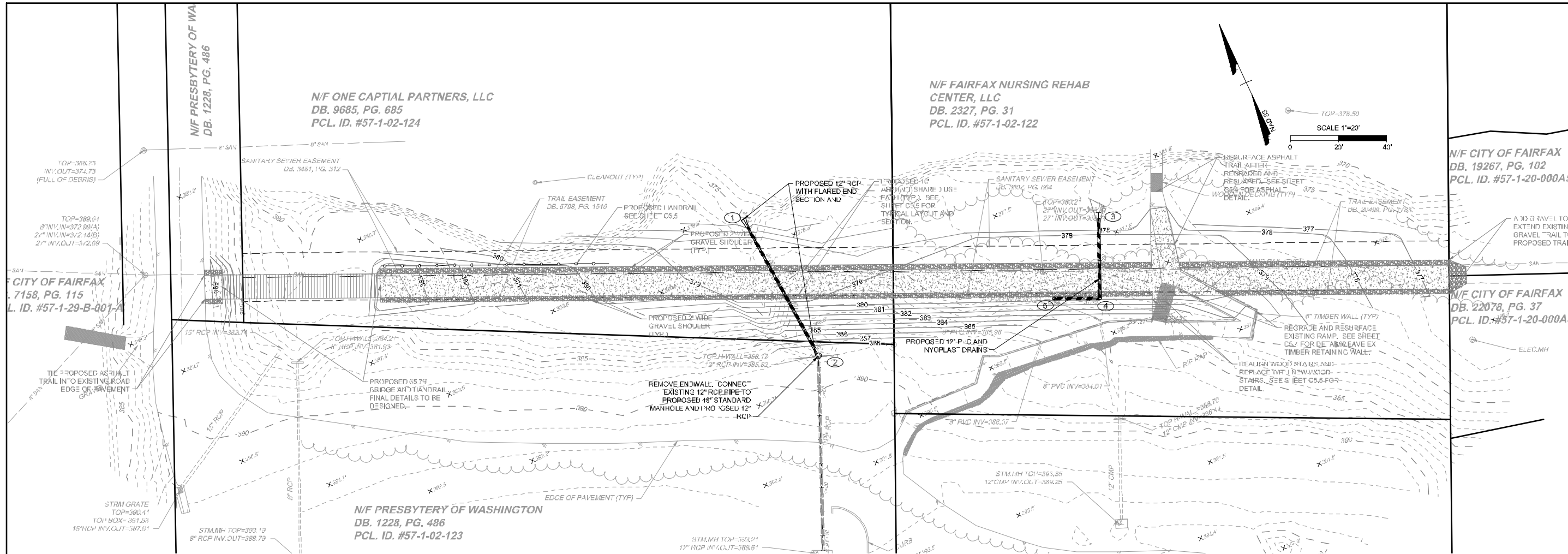
DESIGNED BY: JJD  
DESIGN CHECKED BY: JJD  
CHECKED BY: CMK  
SCALE: SHEET PLAN

JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
JUB NO.: 35061.030  
SHEET NO.: C7.1

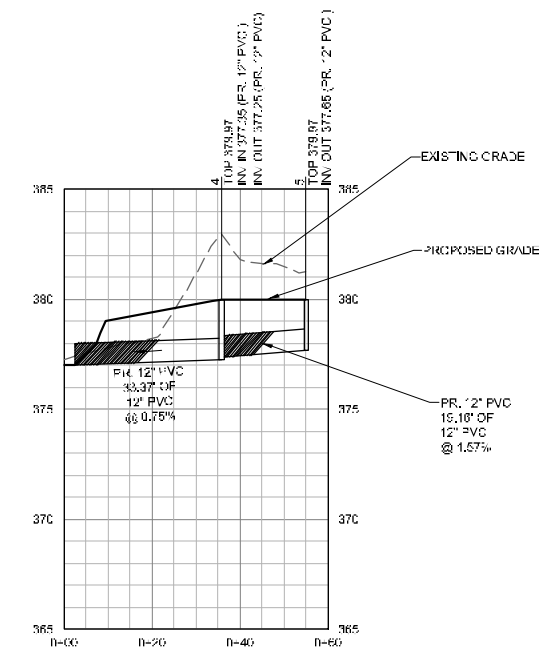


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PROFILE FOR STM 1-2  
SCALE: 1" = 20' HORIZONTAL 1" = 4' VERTICAL

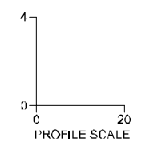


PROFILE FOR STM 3-5  
SCALE: 1" = 20' HORIZONTAL 1" = 4' VERTICAL

**STORM DRAIN NARRATIVE**

AN EXISTING 12" RCP PIPE DRAINS DISCHARGES THROUGH A HEADWALL AND DRAINS ONTO THE SITE. THIS PROJECT PROPOSES TO EXTEND THE EXISTING 12" RCP PIPE APPROXIMATELY SIXTY FEET (60') TO ACCOTINK CREEK. THE EXISTING HEADWALL IS PROPOSED TO BE REMOVED AND REPLACED WITH A STANDARD 48" MANHOLE APPROXIMATELY 13' AWAY. A PROPOSED 12" RCP WILL BE CONNECTED TO THE 48" STANDARD MANHOLE TO CONNECT TO THE EXISTING 12" RCP. PROFILE STM 1-2 PROVIDES DETAILED INFORMATION FOR THE EXISTING AND PROPOSED 12" RCP PIPES AS WELL AS THE 48" MANHOLE AND FLARED END OUTLET STRUCTURE. THE SUBSEQUENT PLAN SHEETS (C8.1 - C8.2) PROVIDE DETAILS FOR EACH OF THESE COMPONENTS.

TWO (2) NYLOPLAST INLETS CONNECTED TO A 12" PVC PIPE ARE PROPOSED ADJACENT TO THE STAIRS AND RAMP TO PROVIDE CONVEYANCE FOR THE EXISTING CURVERS UNDER THE EXISTING DISABLED ACCESSIBILITY RAMP. PROFILE STM 3-5 PROVIDES DETAILED INFORMATION FOR THE NYLOPLAST INLETS AND 12" PVC PIPE. THE SUBSEQUENT PLAN SHEETS (C8.2 - C8.2) PROVIDE DETAILS FOR THE NYLOPLAST INLETS.



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JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA

STORM DRAIN IMPROVEMENTS

JOB NO.: 35061.030

SHEET NO.: C8.0

DATE: 09.22.2017

DRAWN BY: JJD

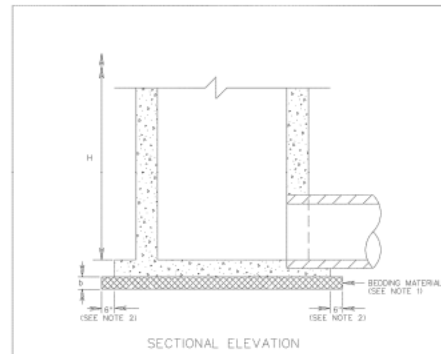
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SCALE: SEE PLAN

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NOTES

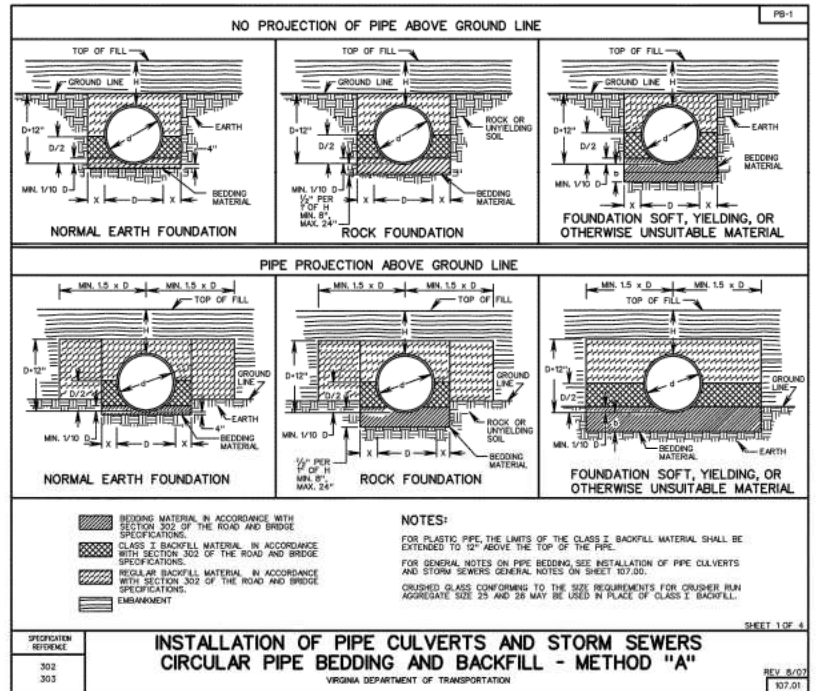
- BEDDING MATERIAL IS TO BE AGGREGATE SIZE 25 OR 26. IF FOUNDATION HAS STANDING OR RUNNING WATER PRESENT, THEN AGGREGATE NO. 57 SHALL BE USED FOR THE DEPTH SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER, CAPPED WITH 4 INCHES OF AGGREGATE NO. 25 OR 26.
- WIDTH OF BEDDING MATERIAL SHALL EXTEND A MINIMUM OF 8" BEYOND THE BASE OF THE STRUCTURE ON ALL SIDES.
- HEIGHT OF STRUCTURE (H) IS MEASURED FROM THE INVERT OF THE STRUCTURE TO THE TOP OF THE FRAME AND COVER OR CONCRETE BEDDING ON STRUCTURE TYPE. SEE APPLICABLE DRAINAGE STRUCTURE STANDARDS FOR DETAIL.

BEDDING THICKNESS TABLE

FOUNDATION TYPE	BEDDING THICKNESS (b)
NORMAL EARTH	4" FOR H ≤ 10' 6" FOR H > 10'
ROCK	1" PER FOOT OF H, MAX. 6"
SOFT & YIELDING	AS SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER

DRAINAGE STRUCTURE BEDDING FOR DROP INLET, MANHOLE, AND JUNCTION BOX

SPECIFICATION REFERENCE: 302  
 VIRGINIA DEPARTMENT OF TRANSPORTATION  
 DATE: 100.15

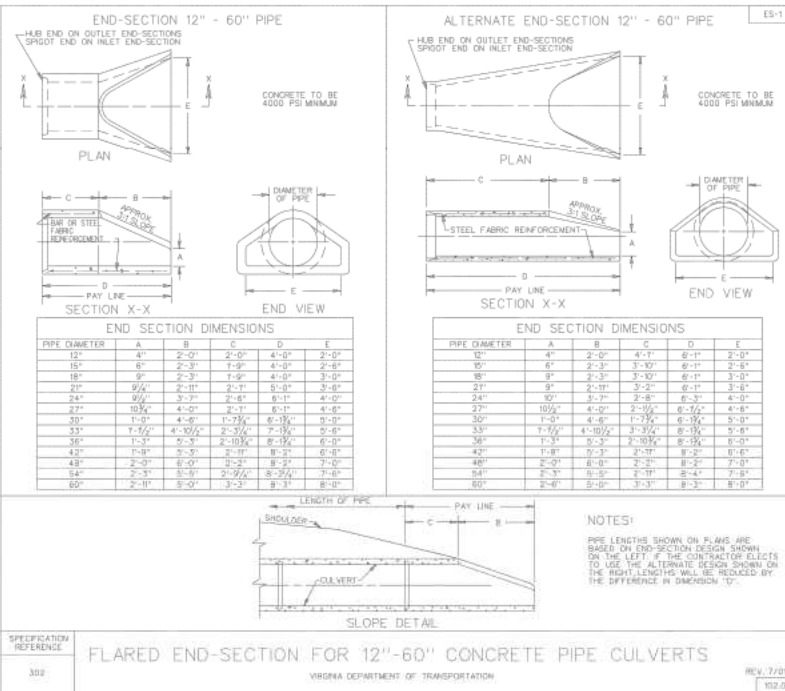


NOTES:

- FOR PLASTIC PIPE, THE LIMITS OF THE CLASS 2 BACKFILL MATERIAL SHALL BE EXTENDED TO 12" ABOVE THE TOP OF THE PIPE.
- FOR GENERAL NOTES ON PIPE BEDDING, SEE INSTALLATION OF PIPE CULVERTS AND STORM SEWERS GENERAL NOTES ON SHEET 103.05.
- CRUSHED GLASS CONFORMING TO THE SIZE REQUIREMENTS FOR CRUISER RUN AGGREGATE SIZE 25 AND 26 MAY BE USED IN PLACE OF CLASS 1 BACKFILL.

INSTALLATION OF PIPE CULVERTS AND STORM SEWERS CIRCULAR PIPE BEDDING AND BACKFILL - METHOD "A"

SPECIFICATION REFERENCE: 302, 303  
 VIRGINIA DEPARTMENT OF TRANSPORTATION  
 DATE: 107.01

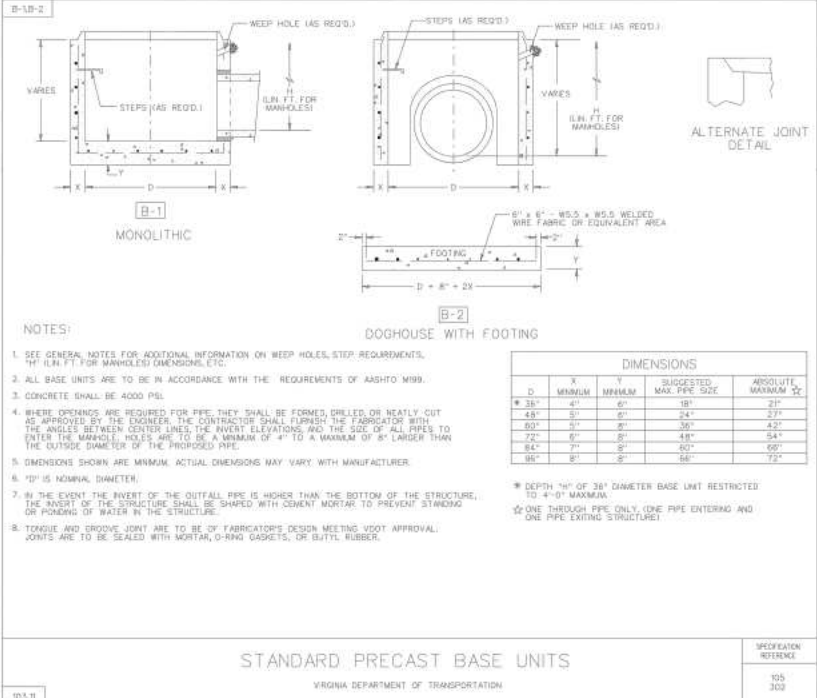


NOTES:

- PIPE LENGTHS SHOWN ON PLANS ARE BASED ON END-SECTION DESIGN SHOWN ON THE LEFT. IF THE CONTRACTOR elects TO USE THE ALTERNATE DESIGN SHOWN ON THE RIGHT, LENGTHS WILL BE REDUCED BY THE DIFFERENCE IN DIMENSION "D".

FLARED END-SECTION FOR 12"-60" CONCRETE PIPE CULVERTS

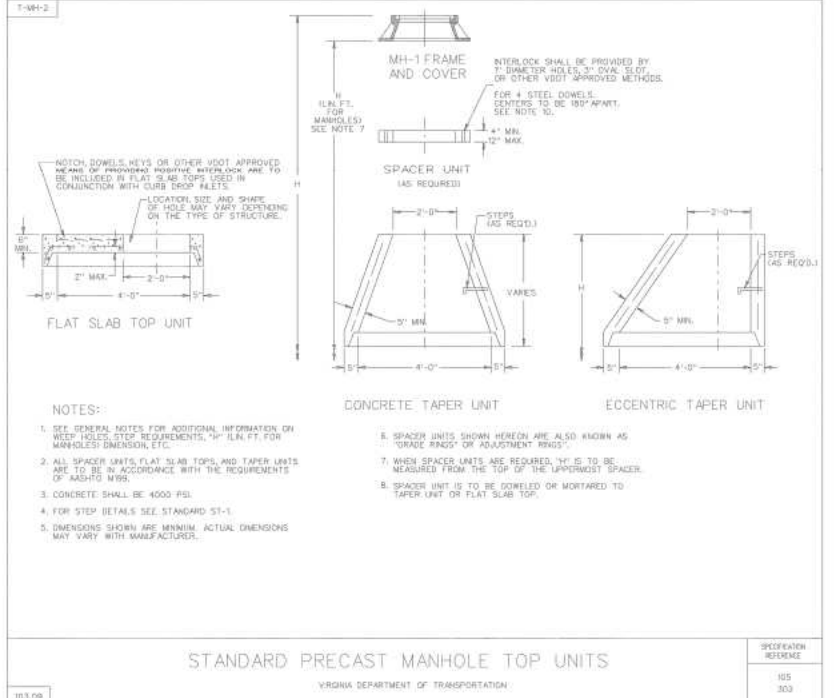
SPECIFICATION REFERENCE: 302  
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D	X	Y	SUGGESTED MAX. PIPE SIZE	ABSOLUTE MAXIMUM SIZE
36"	48"	60"	18"	21"
48"	60"	72"	24"	27"
60"	72"	84"	30"	33"
72"	84"	96"	36"	39"
84"	96"	108"	42"	45"
96"	108"	120"	48"	51"

STANDARD PRECAST BASE UNITS

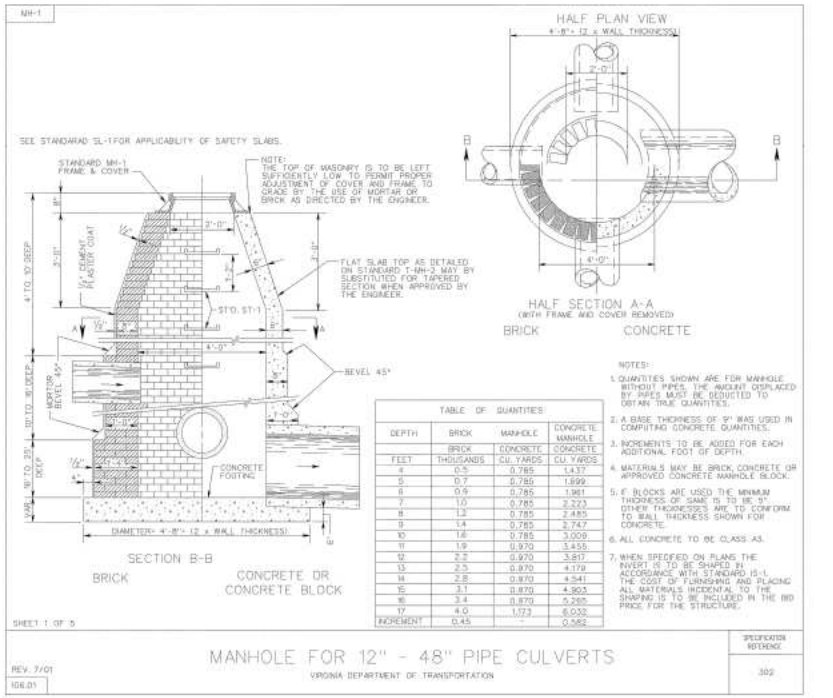
SPECIFICATION REFERENCE: 103, 302  
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 DATE: 103.09



DEPTH	BRICK		CONCRETE	
	THOUSANDS	CU YARDS	THOUSANDS	CU YARDS
4'	0.5	0.785	1.137	1.699
5'	0.7	1.080	1.617	2.323
6'	0.9	1.375	2.097	3.047
7'	1.1	1.670	2.577	3.771
8'	1.3	1.965	3.057	4.495
9'	1.5	2.260	3.537	5.219
10'	1.7	2.555	4.017	5.943
11'	1.9	2.850	4.497	6.667
12'	2.1	3.145	4.977	7.391
13'	2.3	3.440	5.457	8.115
14'	2.5	3.735	5.937	8.839
15'	2.7	4.030	6.417	9.563
16'	2.9	4.325	6.897	10.287
17'	3.1	4.620	7.377	11.011
18'	3.3	4.915	7.857	11.735
19'	3.5	5.210	8.337	12.459
20'	3.7	5.505	8.817	13.183

STANDARD PRECAST MANHOLE TOP UNITS

SPECIFICATION REFERENCE: 103, 303  
 VIRGINIA DEPARTMENT OF TRANSPORTATION  
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MANHOLE FOR 12" - 48" PIPE CULVERTS

SPECIFICATION REFERENCE: 302  
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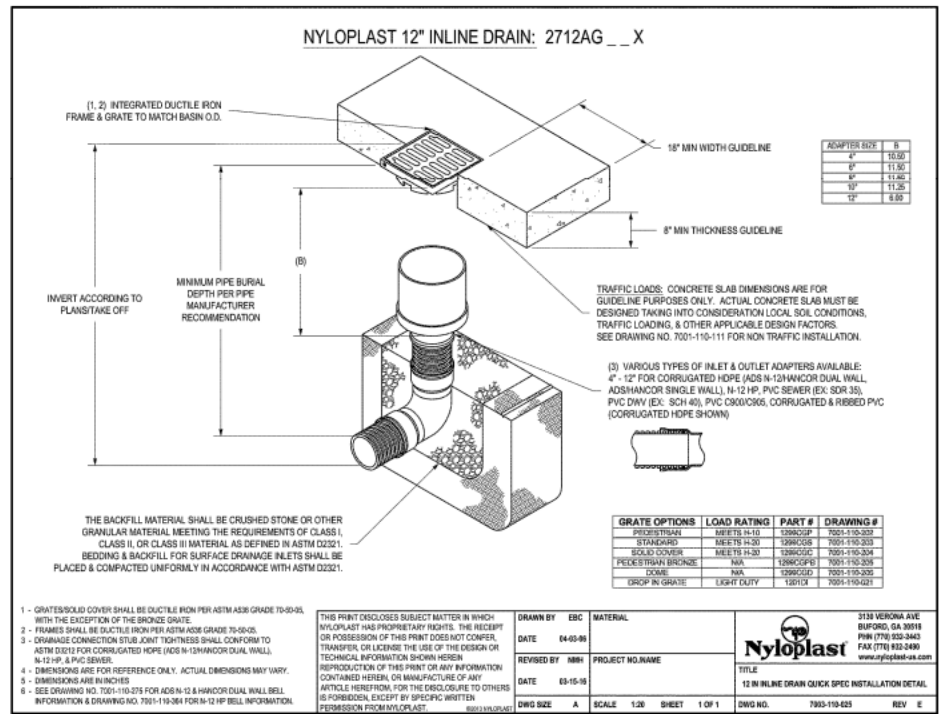
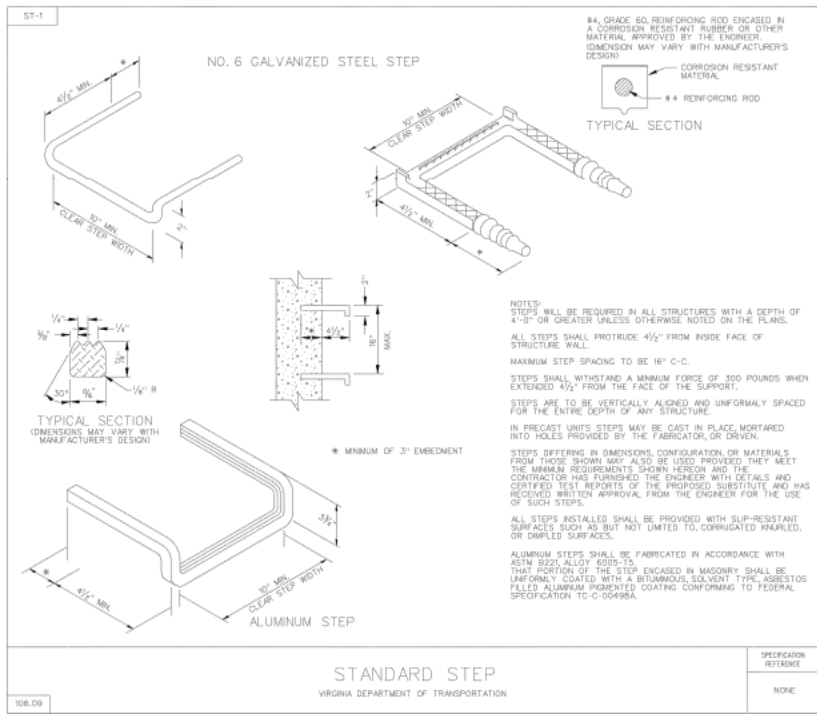
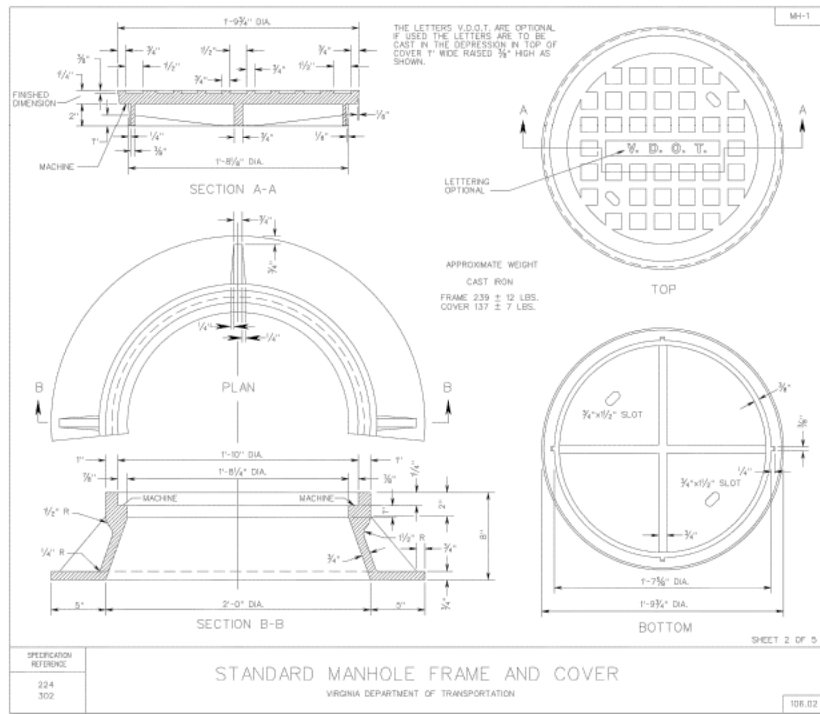
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**TIMMONS GROUP**  
 JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
 STORM DRAIN NOTES AND DETAILS

JOB NO.: 35061.030  
 SHEET NO.: C8.1

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JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
STORM DRAIN NOTES AND DETAILS

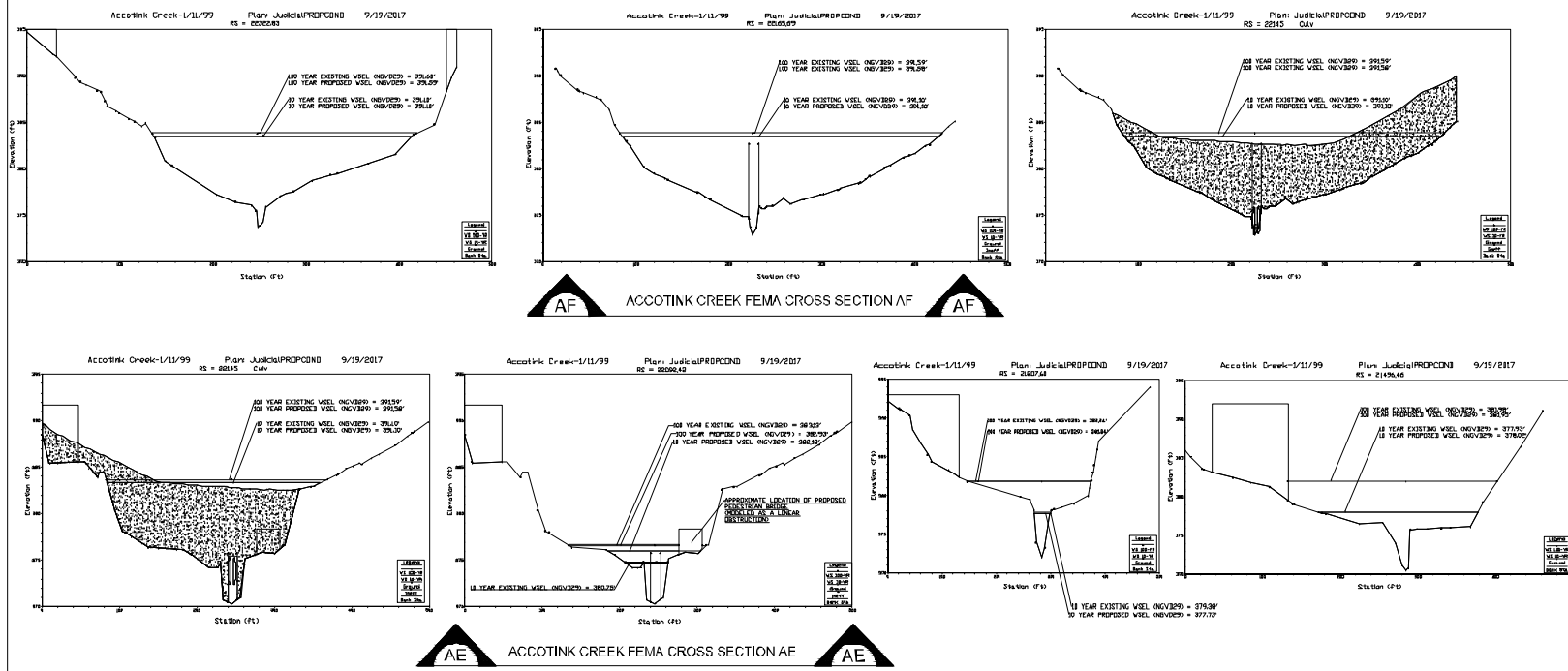
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**DECLARATION OF NO IMPACT TO FLOODPLAIN**  
 THIS PROJECT CONSISTS OF A PROPOSED TRAIL EXTENSION ALONG ACCOTINK CREEK. THE PROPOSED TRAIL EXTENSION IS LOCATED WITHIN THE ACCOTINK CREEK FLOODPLAIN. THE PROPOSED TRAIL EXTENSION IS NOT A STRUCTURE AND DOES NOT IMPROVE THE FLOODPLAIN. THE PROPOSED TRAIL EXTENSION IS NOT A STRUCTURE AND DOES NOT IMPROVE THE FLOODPLAIN. THE PROPOSED TRAIL EXTENSION IS NOT A STRUCTURE AND DOES NOT IMPROVE THE FLOODPLAIN.

**JUDICIAL DRIVE TRAIL EXTENSION  
 ACCOTINK CREEK PROPOSED HEC-RAS CROSS SECTIONS**



**HEC RAS CROSS SECTION - 22322.83**  
 10 YEAR EXISTING WSEL (NGVD29) = 391.10'  
 10 YEAR PROPOSED WSEL (NGVD29) = 391.10'  
 100 YEAR EXISTING WSEL (NGVD29) = 391.60'  
 100 YEAR PROPOSED WSEL (NGVD29) = 391.58'

**ACCOTINK CREEK FEMA CROSS SECTION - AE**  
 HEC RAS CROSS SECTION - 22092.42  
 10 YEAR EXISTING WSEL (NGVD29) = 380.75'  
 10 YEAR PROPOSED WSEL (NGVD29) = 382.18'  
 100 YEAR EXISTING WSEL (NGVD29) = 383.15'  
 100 YEAR PROPOSED WSEL (NGVD29) = 382.92'

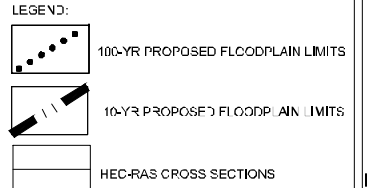
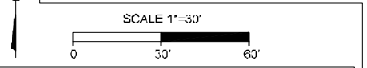
**HEC RAS CROSS SECTION - 21807.61**  
 10 YEAR EXISTING WSEL (NGVD29) = 379.38'  
 10 YEAR PROPOSED WSEL (NGVD29) = 377.73'  
 100 YEAR EXISTING WSEL (NGVD29) = 382.24'  
 100 YEAR PROPOSED WSEL (NGVD29) = 381.84'

**HEC RAS CROSS SECTION - 21496.46**  
 10 YEAR EXISTING WSEL (NGVD29) = 377.93'  
 10 YEAR PROPOSED WSEL (NGVD29) = 378.02'  
 100 YEAR EXISTING WSEL (NGVD29) = 381.98'  
 100 YEAR PROPOSED WSEL (NGVD29) = 381.95'

**EXISTING CONDITIONS VS. PROPOSED CONDITIONS - HEC-RAS PROFILE SUMMARY TABLE**

Profile No.	Profile Name	Flow Rate	Flowline	Flowline	Q Total	Vel (ft/s)	W.S. Elev.	C.S. Elev.	D. Elev.	C.D. Slope	Vel (ft/s)	Flow Area	Top Width	Friction Co.
1	Accotink Creek - US of Chain Bl	22322.83	10-YR	Judicial Drive	379.38	3.11	381.10	381.10	381.10	0.000015	3.11	121.72	117.43	0.28
2	Accotink Creek - US of Chain Bl	22092.42	10-YR	Judicial Drive	382.18	3.11	382.18	382.18	382.18	0.000015	3.11	121.72	117.43	0.28
3	Accotink Creek - US of Chain Bl	21807.61	10-YR	Judicial Drive	377.73	3.11	377.73	377.73	377.73	0.000015	3.11	121.72	117.43	0.28
4	Accotink Creek - US of Chain Bl	21496.46	10-YR	Judicial Drive	378.02	3.11	378.02	378.02	378.02	0.000015	3.11	121.72	117.43	0.28

**THIS SHEET FOR INFORMATION PURPOSES ONLY!!!!**



NOTE: BASE SURVEY CONTOUR DATA HAS BEEN CONVERTED TO NGVD 29 FROM NAVD 88 FOR THE PURPOSES OF THIS STUDY.

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 DATE: \_\_\_\_\_  
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 CHECKED BY: \_\_\_\_\_

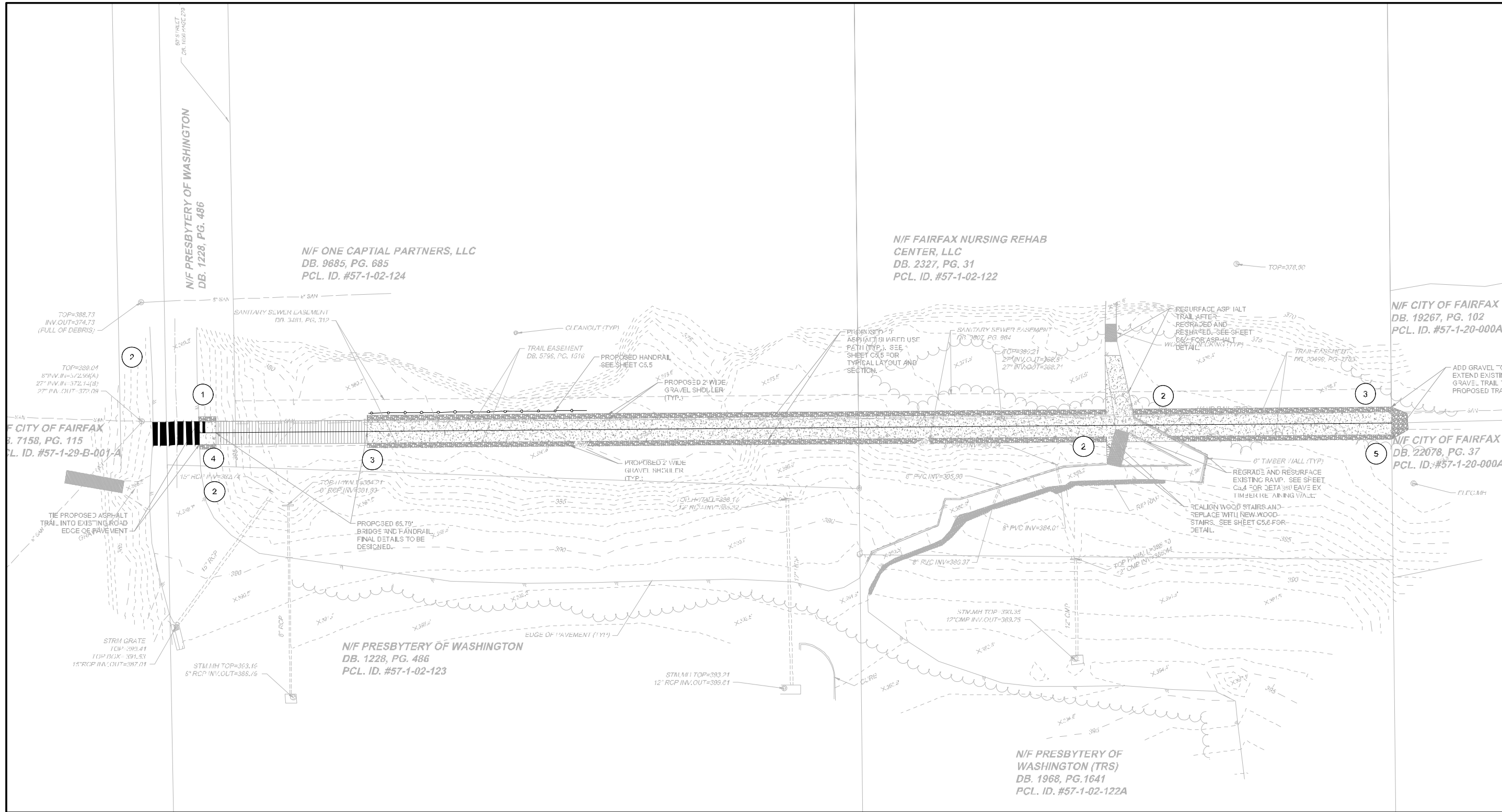
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JUDICIAL DRIVE TRAIL CONNECTION

JOB NO.: \_\_\_\_\_  
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**JUDICIAL DRIVE TRAIL CONNECTION**  
 CITY OF FAIRFAX, VIRGINIA  
**SIGNAGE PLAN**

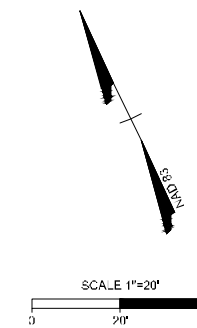
**TIMMONS GROUP**  
 35061.030  
 SHEET NO. C9.0

**SIGNAGE NARRATIVE**

A TOTAL OF NINE (9) SIGNS ARE PROPOSED WITH THE TRAIL. A STOP SIGN IS PROPOSED WHERE THE TRAIL INTERSECTS WITH PRESBYTERIAN WAY. ALONG PRESBYTERIAN WAY TWO (2) TRAIL CROSSING SIGNS ARE PROPOSED NORTH AND SOUTH OF WHERE THE TRAIL INTERSECTS PRESBYTERIAN WAY. ONE (1) STEEP SLOPE SIGN IS PROPOSED AT THE INTERSECTION OF PRESBYTERIAN WAY AND TRAIL INTERSECTION. THE STEEP SLOPE SIGN IS PLACED FOR EASTWARD BOUND TRAVELERS. TWO (2) MULTIMODAL SIGNS ARE PROPOSED. THE FIRST MULTIMODAL TRAIL SIGN IS PROPOSED APPROXIMATELY 20' EAST OF THE INTERSECTION OF THE TRAIL AND PRESBYTERIAN WAY AND THE OTHER MULTIMODAL SIGN IS PROPOSED ADJACENT TO THE EASTERN END OF THE PROPOSED TRAIL IMPROVEMENTS. TWO (2) ADDITIONAL TRAIL CROSSING SIGNS ARE PROPOSED WHERE THE RAMP AND STAIRS LEADING FROM THE ADJACENT PARKING LOT AT PRESBYTERIAN WAY TO THE FAIRFAX NURSING CENTER BUILDING. A PAVEMENT ENDS SIGN IS LOCATED ADJACENT TO EASTERN END OF THE TRAIL IMPROVEMENTS WHERE THE TRAIL TRANSITIONS FROM ASPHALT TO GRAVEL.

**MARKINGS NARRATIVE**

THREE (3) MARKINGS ARE PROPOSED ON THE PROJECT SITE. THE FIRST MARKING IS THE TRAIL CENTERLINE. THE CENTERLINE IS TO BE MARKED IN ACCORDANCE WITH THE SIGN AND USE PATH STANDARDS AND SPECIFICATIONS. THE SECOND MARKING IS A STOP BAR WHERE THE TRAIL INTERSECTS WITH PRESBYTERIAN WAY. THE FINAL MARKING IS A CROSSWALK ACROSS PRESBYTERIAN WAY.



**SIGNAGE**

NO.	SIGN	CODE	COUNT
1	STOP	R1-1	1
2	TRAIL CROSSING	W-11-1uP	4
3	TRAIL CROSSING	W11-15	2
4	STEEP SLOPE	W7-5	1
5	PAVEMENT ENDS	W6-5	1
	TOTAL		9

**MARKINGS**

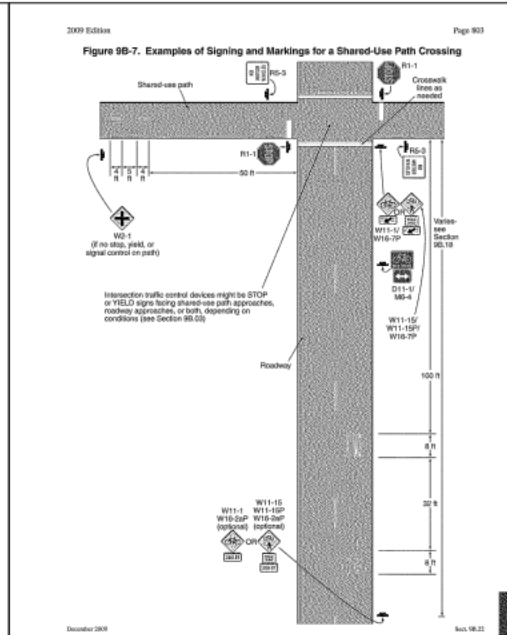
MARKING	DESCRIPTION	QUANTITY
[Symbol: Stop bar]	STOP BAR	1
[Symbol: Centerline]	CENTERLINE	616 FT
[Symbol: Crosswalk]	CROSSWALK	20 FT



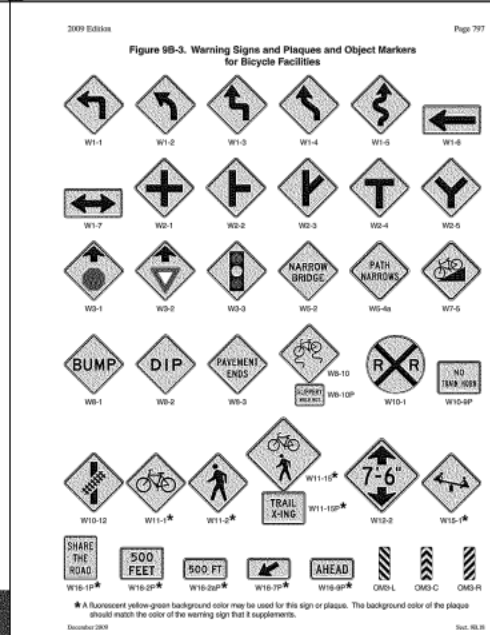
2009 Edition Page 789 PART 9 CHAPTER 9A. GENERAL SECTION 9A.01 Requirements for Bicycle Traffic Control Devices...

Page 790 Table 9B-1. Bicycle Facility Sign and Plaque Minimum Sizes (Sheet 1 of 2)...

Page 790 Section 9B.13 Bicycle Signal Activation Sign (R18-22) Section 9B.14 Other Regulatory Signs...



Page 790 CHAPTER 9B. SIGNS SECTION 9B.01 Application and Placement of Signs...

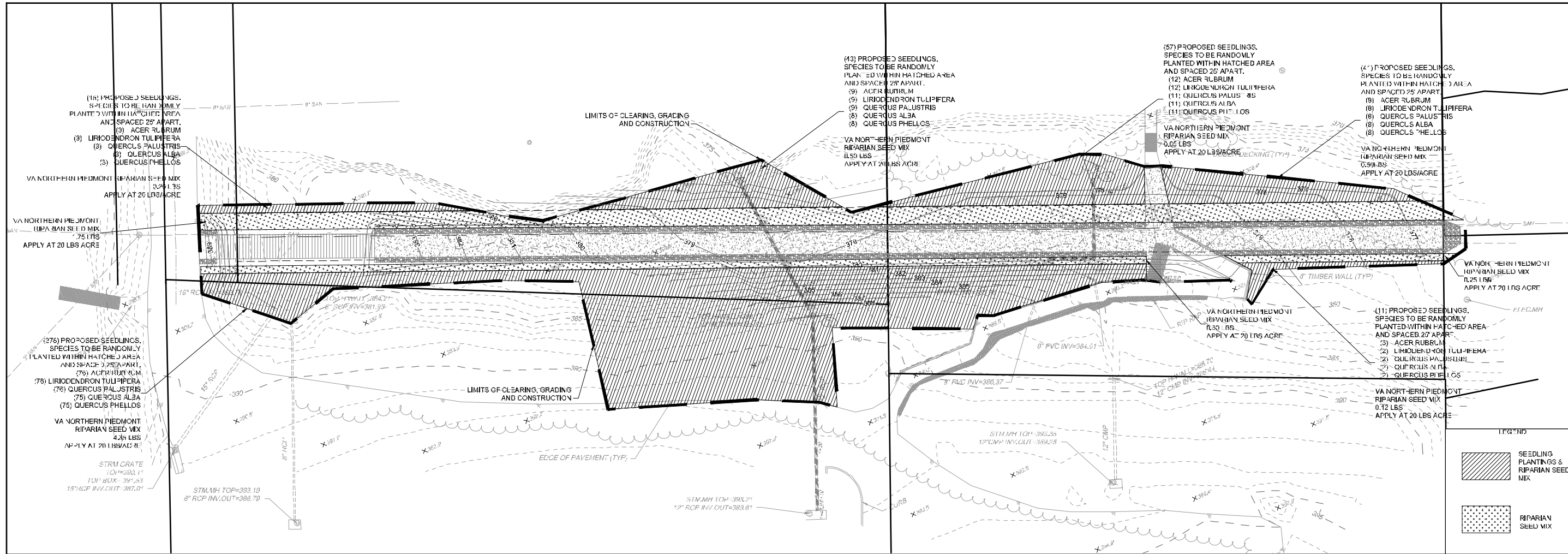


Page 791 Table 9B-1. Bicycle Facility Sign and Plaque Minimum Sizes (Sheet 2 of 2)...

Page 794 Section 9B.04 Bike Lane Signs and Plaques (R3-17, R3-17A, R3-17B) Section 9B.05 BEGIN RIGHT TURN LANE YIELD TO BIKES Sign (R4-1)...

Page 796 Section 9B.06 Bicycles May Use Full Lane Sign (R4-1L) Section 9B.07 Bicycles May Use Full Lane Sign (R4-1L)...

JUDICIAL DRIVE TRAIL CONNECTION CITY OF FAIRFAX, VIRGINIA SIGNAGE NOTES AND DETAILS TIMMONS GROUP INC. YOUR VISION ACHIEVED THROUGH OURS. JOB NO. 35061.030 SHEET NO. C9.1



**LANDSCAPE NARRATIVE**

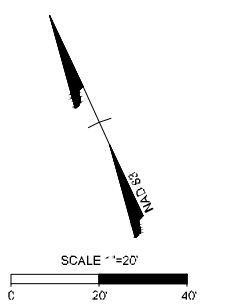
A THREE FOOT (3') STRIP ADJACENT TO THE "WOOD" (2') GRAVEL SHOULDERS IS PROPOSED TO BE REPLANTED WITH THE ERNST VIRGINIA NORTHERN PIEDMONT RIPARIAN MIX. THE ERNST VIRGINIA NORTHERN PIEDMONT RIPARIAN MIX IS PROPOSED TO BE PLANTED THROUGHOUT THE ENTIRE SITE IN ADDITION TO A STRIP ADJACENT PERMANENT SEEDLING MIXTURE. A TOTAL OF 5,522 POUNDS IS REQUIRED TO SEED THIS AREA.

A TOTAL OF 545 SEEDLING TREES ARE PROPOSED WITHIN THE DISTURBED AREAS. THE SEEDLING TREES ARE NATIVE RIPARIAN TREES TYPICALLY FOUND WITHIN THE CITY LIMITS.

**PLANT SCHEDULE**

QUANTITY	SCIENTIFIC NAME	COMMON NAME	INDICATOR STATUS	SIZE AT PLANTING	SPACING	NATIVE	PLANT NOTES
112	<i>Acer rubrum</i>	Red Maple	FAC	Tubeling/Bare Root Seedling	25'	YES	Randomly plant throughout designated area.
110	<i>Liriodendron tulipifera</i>	Tulip Poplar	FACW	Tubeling/Bare Root Seedling	25'	YES	
109	<i>Quercus alba</i>	White Oak	FACU	Tubeling/Bare Root Seedling	25'	YES	
107	<i>Quercus palustris</i>	Pin Oak	FACW	Tubeling/Bare Root Seedling	25'	YES	
107	<i>Quercus phellos</i>	Willow Oak	FAC	Tubeling/Bare Root Seedling	25'	YES	
<b>Virginia Northern Piedmont Riparian Mix</b>							
22%	<i>Panicum clandestinum</i>	Dichanthelium	FAC	Seed Mix	N/A	YES	Seed mix to be spread at rate of 20 lbs/acre. Virginia Northern Region Seed Mix from Ernst Seeds. See Landscape Plan for total seed mix requirements.
15%	<i>Elymus virginicus</i> , 'Suther'	Virginia Wildrye, 'Suther'	FACW	Seed Mix	N/A	YES	
15%	<i>Sorghastrum nutans</i>	Indiangrass	FACW	Seed Mix	N/A	YES	
10%	<i>Panicum anceps</i>	Beaked Panicgrass	FAC	Seed Mix	N/A	YES	
10%	<i>Poa palustris</i>	Fowl Bluegrass	FACW	Seed Mix	N/A	YES	
8%	<i>Andropogon gerrardii</i>	Big Bluestem Niagra	FAC	Seed Mix	N/A	YES	
5%	<i>Panicum virgatum</i> , 'Blackwell'	Blackwell Switchgrass	FAC	Seed Mix	N/A	YES	
2%	<i>Agrostis perennans</i>	Autumn Bentgrass	FACU	Seed Mix	N/A	YES	
2%	<i>Eupatorium fistulosum</i>	Joe Pye Weed	FACW	Seed Mix	N/A	YES	
2%	<i>Eupatorium perfoliatum</i>	Boneset	FACW	Seed Mix	N/A	YES	
2%	<i>Helenium autumnale</i>	Common Sneezeweed	FACW	Seed Mix	N/A	YES	
2%	<i>Senna herbicarpa</i>	Cassia	FAC	Seed Mix	N/A	YES	
2%	<i>Verbena hastata</i>	Blue Vervain	FACW	Seed Mix	N/A	YES	
2%	<i>Vernonia noveboracensis</i>	New York Ironweed	FACW	Seed Mix	N/A	YES	
1%	<i>Hibiscus moscheutos</i>	Crimson-eyed Rosemallow	OBL	Seed Mix	N/A	YES	

Note: Seed mix from Ernst Seed. Item Number ERNMX-852. Seeding Rate = 20 lbs/acre. Mix formula may change depending on seed availability.



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**TIMMONS GROUP**  
JUDICIAL DRIVE TRAIL CONNECTION  
CITY OF FAIRFAX, VIRGINIA  
TREE MANAGEMENT PLAN

JOB NO.: 35061.030  
SHEET NO.: 1.1.0

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1. Soak roots in water 3 to 6 hours prior to planting. Do not allow roots to dry out before planting.

2. Remove grass from a 3 ft circle and turn up soil. Dig a planting hole 8 in wider than the diameter of the seedling roots in center of circle.

3. Position seedling at original nursery depth. Do not bend roots on sides or bottom of planting hole. Gently backfill excavated soil around roots.

4. Remove air pockets from backfill soil. Soil should be firm but not tightly packed. Construct waterholding basin around planting hole and water thoroughly.

5. Place a 2 in deep layer of mulch in a 3 ft diameter circle around trees. Mulch should not touch the tree trunk.

6. During dry weather, water generously once every 7 to 10 days during the first year.

Illustrations provided by The National Arbor Foundation

Ref. Sec. 12-0705.5F Rev. 2-02, 2011 Reprint	PLANTING TREE SEEDLINGS	PLATE NO. 8-12	STD. NO.
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**I. General**

- Landscape Specifications:** Landscape specifications shall be as outlined below. Any item or procedure not mentioned below shall be as specified in the Landscape specification guidelines published by the landscape Contractors Association (latest edition).
- Plant Materials:** The landscape Contractor shall furnish and install and/or dig, ball, burlap, and transplant all of the plant materials called for on the drawings and/or listed in the plant schedule.
- Plant Names:** Plant names used in the plant schedule shall be identified in accordance with Hortus Third, by L.H. Bailey, 1976.
- Plant Standards:** All plant materials shall be equal to or better than the requirements of the "American Standard for Nursery Stock," latest edition, as published by the American Association of Nurserymen (hereinafter referred to as AAN standards). All plants shall be typical of their species and variety, shall have a normal habit of growth, and shall be first quality, sound, vigorous, well branched, and with healthy, well-furnished root systems. They shall be free of disease, insect pests, and mechanical injuries.
  - All plants shall be nursery grown and shall have been grown under the same climatic conditions as the location of the subject project for at least two years before planting. Neither heeled-in plant, nor plants from cold storage will be accepted.
  - Collected plants or transplanted trees may be called for by the landscape architect and used, provided, however, that locations and soil conditions will permit proper balling.
- Materials for Planting:**
  - Stakes for buying trees shall be sound oak or other approved hardwood. Three stakes spread 120-degrees apart shall be used near tree. See details.
  - Wrapping materials for tree trunks: Clean burlap of 8-oz. weight cut in 8-inch to 10-inch wide strips of water resistant paper or tape for this purpose. Twine for tying shall be medium jute twine.
  - Tree Guys: Provide wire ties and guys of 2-strand, twisted, pliable galvanized steel wire not lighter than 12-gauge with zinc coated turnbuckles. Provide w-ply garden hose not less than 0.5-inch hose size, cut to lengths to protect tree trunks from damage by wires.
  - Mulching: Mulch shall consist of double shredded hardwood mulch.
- Planting Schedule:** A professional horticulturist/nurseryman shall be consulted to determine the proper time, based on plant species and weather conditions, to move and install particular plant materials to minimize stress to the plant. Planting of deciduous material may be continued during the winter months provided there is no frost on the ground and frost-free soil planting mixtures are used.

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	<b>PLANTING NOTES DETAIL</b>	SCALE: Not To Scale REVISION DATE: July, 2005 DETAIL # 921.06 SHEET # 1 of 2

**II. Planting Execution**

- Excavation of Plant Pits**
  - Circular Pits, with vertical sides shall be excavated for all plants. The diameter of the holes shall be 12-inches greater than the diameter of the ball for trees, or 1.5 times the diameter of shrubs, balls, and container stock pots.
  - The depth of pits for all plants shall be 6-inches deeper than the ball or container depth.
  - Obstructions encountered in excavated or planted areas shall be removed or plants relocated as approved.
  - Plants shall be planted plumb, at the same grade as in the nursery (in relation to finished grade), tamp topsoil under and around base of ball to fill all voids. Remove all burlap, ropes, and wires from sides and tips of balls, but do not remove burlap from under ball. Thoroughly water when hole is two-thirds full of topsoil. After watering, 3" of mulch shall be applied over a 4" earth berm to create a shallow watering basin around the tree.
  - All shrubs to be planted in conformance with deciduous shrub planting detail 901.04.
- Staking, Guying and Wrapping**
  - Each tree or evergreen shall be immediately staked or guyed.
    - Deciduous tree 2-2.5" caliper or larger and all evergreen trees shall be staked and guyed with three 2" x 2" x 6' hardwood staked per tree, spread 120-degrees apart. All hardwood stakes are to be driven no less than two feet below planting grade and at ten feet from the tree. A double strand of 12-gauge galvanized wire shall be twisted and threaded through 0.5-inch garden hose to protect the tree trunk and secured to the hardwood stake.
    - The 12-gauge galvanized wire shall be placed at a 45-degree angle from the tree to the stake. See deciduous tree with typical guying planting detail.
  - Wrap all deciduous trees. Wrapping shall extend from ground line to second branch. Overlap tree wrap by 50 percent.
- Plant Pruning, Edging, and Mulching**
  - Pruning shall be by experienced landscape contractors. Remove broken or damaged branches and roots. Cut back and thin deciduous material to retain two-thirds of the initial branches. Cut back evergreens to give compact uniform appearance. Damaged or pruned tree leaders shall be cause for rejection.
  - If foliage is present on deciduous plant material, they shall be sprayed with an anti-desiccant, which slows down the transpiration process, through reducing the danger of dehydration.
  - The areas around isolated plants shall be edged and cultivated to the full diameter of the pit.
  - After cultivation, all plant materials shall be mulched with a three-inch layer of double shredded mulch over the entire area of the bed or saucer.

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**TIMMONS GROUP**  
 JUDICIAL DRIVE TRAIL CONNECTION  
 CITY OF FAIRFAX, VIRGINIA  
 LANDSCAPING PLAN  
 JOB NO.: 35061.030  
 SHEET NO.: L1.1

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