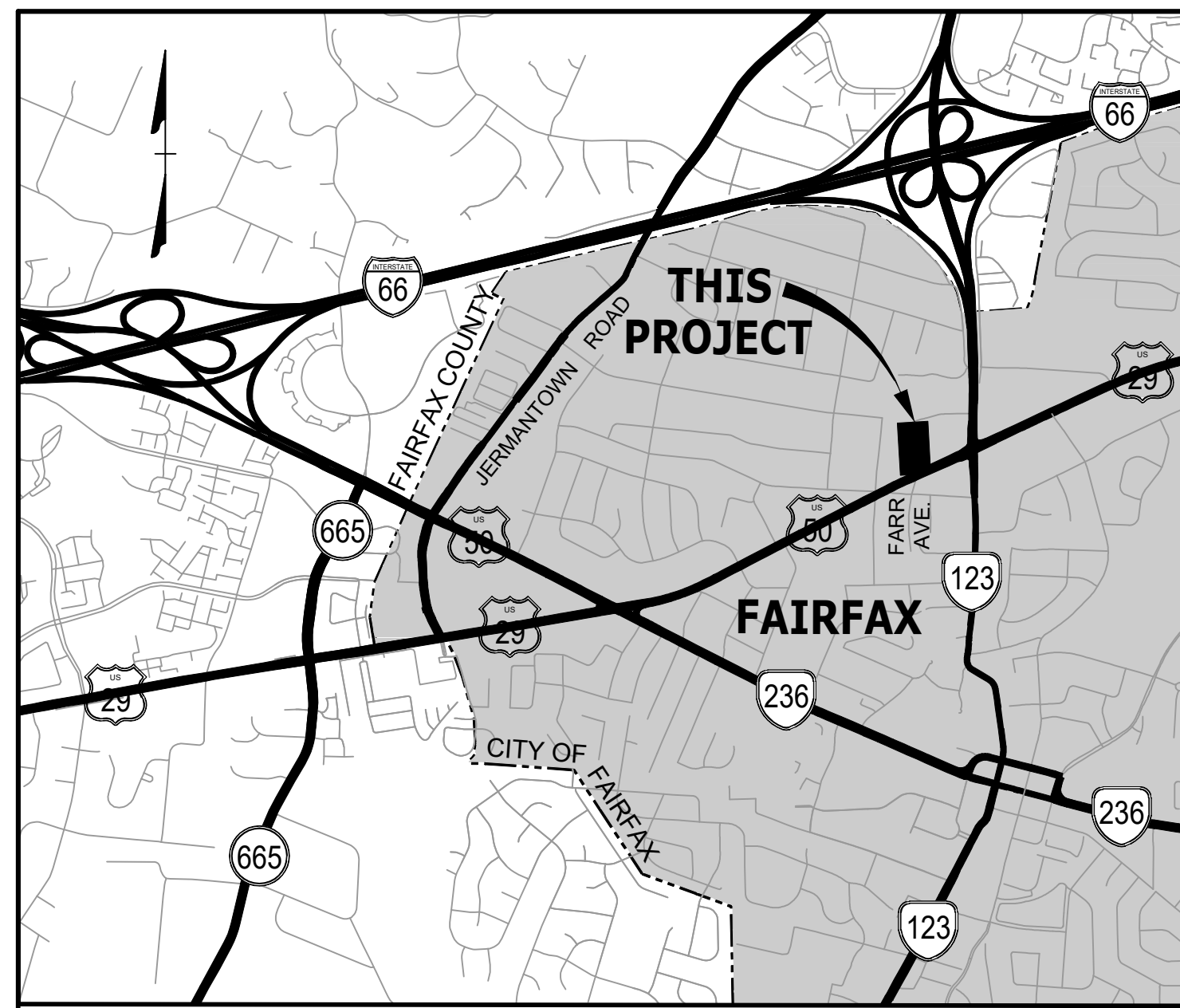


PROJECT MANAGER: CHRISTINA ALEXANDER (CITY OF FAIRFAX) 703-273-3067
 SURVEYED BY, DATE: TIMMONS GROUP, 08/31/17
 DESIGN BY: TIMMONS GROUP
 SUBSURFACE UTILITY BY, DATE: TIMMONS GROUP, 08/31/17



LOCATION MAP
 SCALE: 1" = 2,000'

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TOTAL NUMBER OF SHEETS	27

* DENOTES SHEETS INTENTIONALLY OMITTED FROM THIS SUBMITTAL

CONVENTIONAL SIGNS
 SEE SURVEY AND DESIGN
 LEGEND SYMBOLS ON SHEET 02A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT.

THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DEPARTMENT'S 2016 ROAD AND BRIDGE SPECIFICATIONS, 2016 ROAD AND BRIDGE STANDARDS (INCLUDING UPDATES), 2009 MUTCD, 2011 VIRGINIA SUPPLEMENT TO THE MUTCD, 2011 VIRGINIA WORK AREA PROTECTION MANUAL, REVISION 1, AND AS AMENDED BY CONTRACT PROVISIONS AND THE COMPLETE ELECTRONIC PDF VERSION OF THE PLAN ASSEMBLY.

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THIS PROJECT WAS DEVELOPED UTILIZING THE ENGINEERING DESIGN PACKAGE (AutoCAD CIVIL 3D).
 Computer Identification No. (UPC 113124)



COMMONWEALTH OF VIRGINIA



PLAN AND PROFILE OF PROPOSED STATE HIGHWAY
CITY OF FAIRFAX, VIRGINIA
FARR AVENUE EXTENSION (NORTHFAX WEST)
 FROM: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.)
 TO: WESTERN TERMINUS OF ORCHARD ST.
 (38°51'27" N, 77°18'37" W)

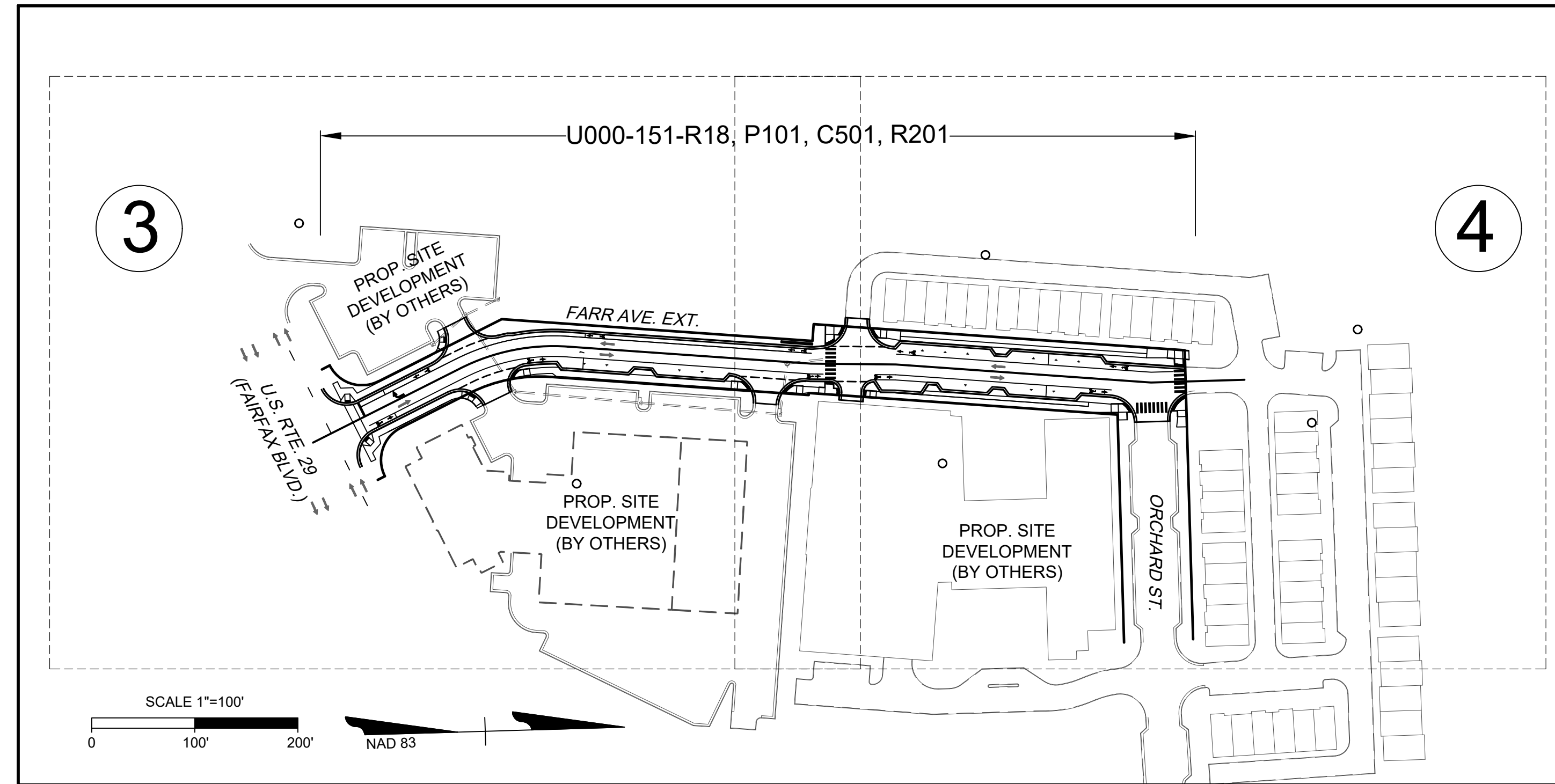
PUBLIC HEARING PLANS

FHWA-534 DATA 27001

STATE	FEDERAL AID PROJECT	ROUTE	STATE PROJECT	SHEET NO.
VA.	SEE TABULATION BELOW FOR SECTION NUMBERS	000	U000-151-R18, P101, R201, C501 (SEE TABULATION BELOW FOR SECTION NUMBERS)	01

FUNCTIONAL CLASSIFICATION AND TRAFFIC DATA	
GS-8 URBAN LOCAL STREET - 25 MPH MINIMUM DESIGN SPEED	
Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.) To: WESTERN TERMINUS OF ORCHARD ST.	
ADT (2018)*	--
ADT (2038)*	--
DHV*	--
D (%) (design hour)*	--
T (%) (design hour)*	--
V (MPH)	25 mph

PLANS SUBJECT TO CHANGE
 FOR INFORMATION ONLY
 NOT FOR CONSTRUCTION



DESCRIPTION REFERENCE
 INTERSECTION OF FARR AVE. AND
 FAIRFAX BLVD. (RTE. 29)
 POI STA. 10+00.00

04/28/2020

Population 24,097 (U.S. Census Bureau - 2017)

STATE PROJECT NO.	SECTION	FEDERAL AID PROJECT NO.	TYPE CODE	UPC NO.	EQUALITIES	LENGTH INCLUDING BRIDGE(S)		LENGTH EXCLUDING BRIDGE(S)		BRIDGE PROJECT NO.	TYPE PROJECT	DESCRIPTION
						FEET	MILES	FEET	MILES			
U000-151-R18	P101	--	PENG	113124	NONE	874.27	0.166	874.27	0.166		PRELIMINARY	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.) To: WESTERN TERMINUS OF ORCHARD ST.
	R201	--	ROW	113124	NONE	874.27	0.166	874.27	0.166		RIGHT OF WAY	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.) To: WESTERN TERMINUS OF ORCHARD ST.
	C501	--	CONSTR	113124	NONE	824.27	0.166	874.27	0.166		CONSTR.	Fr: INTERSECTION OF FARR AVE. AND U.S. RTE. 29 (FAIRFAX BLVD.) To: WESTERN TERMINUS OF ORCHARD ST.

Project Lengths are based on Construction Baseline.

TIER 1 PROJECT

LOCALLY ADMINISTERED PROJECTS	
NAME OF LOCALITY	
(SIGNATURE)	
NAME OF RESPONSIBLE LOCAL GOVERNMENT OFFICIAL (TYPED)	
RECOMMENDED FOR APPROVAL FOR RIGHT OF WAY ACQUISITION	
DATE	TITLE OF POSITION
(SIGNATURE)	
NAME OF RESPONSIBLE LOCAL GOVERNMENT OFFICIAL (TYPED)	
RECOMMENDED FOR APPROVAL FOR CONSTRUCTION	
DATE	TITLE OF POSITION

PROPOSED ROADWAY
CONSTRUCTION ALIGNMENT DATA

SURVEY CONTROL DATA

PLANS SUBJECT
TO CHANGE

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CONSTRUCTION

BENCHMARK DATA TO BE PROVIDED FOR CONSTRUCTION STAKEOUT.
ADDITIONAL INFORMATION TO BE PROVIDED WITH SUBSEQUENT SUBMITTALS.

Alignment Name: Fairfax Blvd.
Station Range: Start: 20+00.00, End: 23+18.54

Begin Fairfax Blvd.
N 6,997,413.2460 E 11,821,573.5800 20+00.00

Line (1)
N64° 57' 52.57"E 318.544'
N 6,997,548.0470 E 11,821,862.1960 23+18.54
Line (1)

N 6,997,548.0470 E 11,821,862.1960 23+18.54
End Fairfax Blvd.

Alignment Length: 318.544'

Alignment Name: Farr Ave Ext
Station Range: Start: 10+00.00, End: 19+25.56

Begin Farr Ave Ext
N 6,997,463.0385 E 11,821,726.8751 10+00.00

Line (1)
N25° 06' 29.50"W 160.345'
N 6,997,608.2326 E 11,821,658.8359 11+60.35
Line (1)

Curve (2)
BC N 6,997,608.2326 E 11,821,658.8359 11+60.35
CTR N 6,997,693.0984 E 11,821,839.9376
PI N 6,997,658.1272 E 11,821,635.4549

Direction Back N25° 06' 29.50"W
Radius 200.000'
Delta 30°48'23"(RT)
Length 107.535'
Tangent 55.101'
Chord Direction N9° 42' 18.00"W Distance 106.244'
Direction Ahead N5° 41' 53.50"E

EC N 6,997,712.9561 E 11,821,640.9258 12+67.88
Curve (2)

Line (3)
N5° 41' 53.50"E 236.339'
N 6,997,948.1269 E 11,821,664.3915 15+04.22
Line (3)

Line (4)
Non-Tangent Radial Bearing N84° 18' 06.50"W
N2° 13' 29.62"E 82.531'
N 6,998,030.5955 E 11,821,667.5955 15+86.75
Line (4)

Line (5)
N5° 41' 53.50"E 248.112'
N 6,998,277.4816 E 11,821,692.2302 18+34.86
Line (5)

Line (6)
Non-Tangent Radial Bearing N84° 18' 06.50"W
N0° 00' 00.00"E 90.703'
N 6,998,368.1848 E 11,821,692.2302 19+25.56
Line (6)

N 6,998,368.1848 E 11,821,692.2302 19+25.56
End Farr Ave Ext

Alignment Length: 925.565'

Alignment Name: Entr. #1
Station Range: Start: 100+00.00, End: 100+60.24

Begin Entr. #1
N 6,997,621.9380 E 11,821,653.0253 100+00.00

Line (1)
S64° 53' 30.50"W 60.239'
N 6,997,596.3770 E 11,821,598.4788 100+60.24
Line (1)

N 6,997,596.3770 E 11,821,598.4788 100+60.24
End Entr. #1

Alignment Length: 60.239'

Alignment Name: Entr. #2
Station Range: Start: 200+00.00, End: 200+63.17

Begin Entr. #2
N 6,997,626.3257 E 11,821,651.4133 200+00.00

Line (1)
N65° 51' 20.52"E 63.174'
N 6,997,652.1660 E 11,821,709.0604 200+63.17
Line (1)

N 6,997,652.1660 E 11,821,709.0604 200+63.17
End Entr. #2

Alignment Length: 63.174'

Alignment Name: Entr. #3
Station Range: Start: 300+00.00, End: 300+62.35

Begin Entr. #3
N 6,997,907.5138 E 11,821,660.3391 300+00.00

Line (1)
S84° 18' 06.50"E 62.353'
N 6,997,901.3229 E 11,821,722.3840 300+62.35
Line (1)

N 6,997,901.3229 E 11,821,722.3840 300+62.35
End Entr. #3

Alignment Length: 62.353'

Alignment Name: Entr. #4
Station Range: Start: 400+00.00, End: 400+80.38

Begin Entr. #4
N 6,997,990.1289 E 11,821,666.0233 400+00.00

Line (1)
N84° 18' 06.50"W 80.381'
N 6,997,998.1098 E 11,821,586.0397 400+80.38
Line (1)

N 6,997,998.1098 E 11,821,586.0397 400+80.38
End Entr. #4

Alignment Length: 80.381'

Alignment Name: Entr. #5
Station Range: Start: 500+00.00, End: 500+67.98

Begin Entr. #5
N 6,997,989.5890 E 11,821,666.0024 500+00.00

Line (1)
S84° 02' 10.14"E 67.980'
N 6,997,982.5259 E 11,821,733.6139 500+67.98
Line (1)

N 6,997,982.5259 E 11,821,733.6139 500+67.98
End Entr. #5

Alignment Length: 67.980'

Alignment Name: Orchard St.
Station Range: Start: 600+00.00, End: 600+79.88

Begin Orchard St.
N 6,998,277.4816 E 11,821,692.2302 600+00.00

Line (1)
N90° 00' 00.00"E 79.883'
N 6,998,277.4816 E 11,821,772.1131 600+79.88
Line (1)

N 6,998,277.4816 E 11,821,772.1131 600+79.88
End Orchard St.

Alignment Length: 79.883'

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REVISION DESCRIPTION

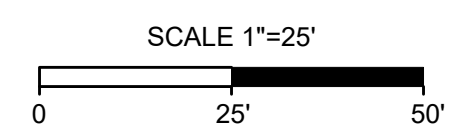
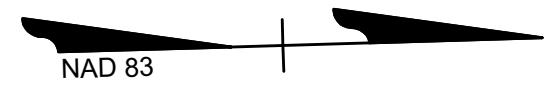
DATE
04/28/2020
DRAWN BY
IJH
DESIGNED BY
IJH
CHECKED BY
NJS
SCALE
1"=25'

TIMMONS GROUP
FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
ALIGNMENT & SURVEY DATA SHEET
JOB NO.
41013.009
SHEET NO.
01A

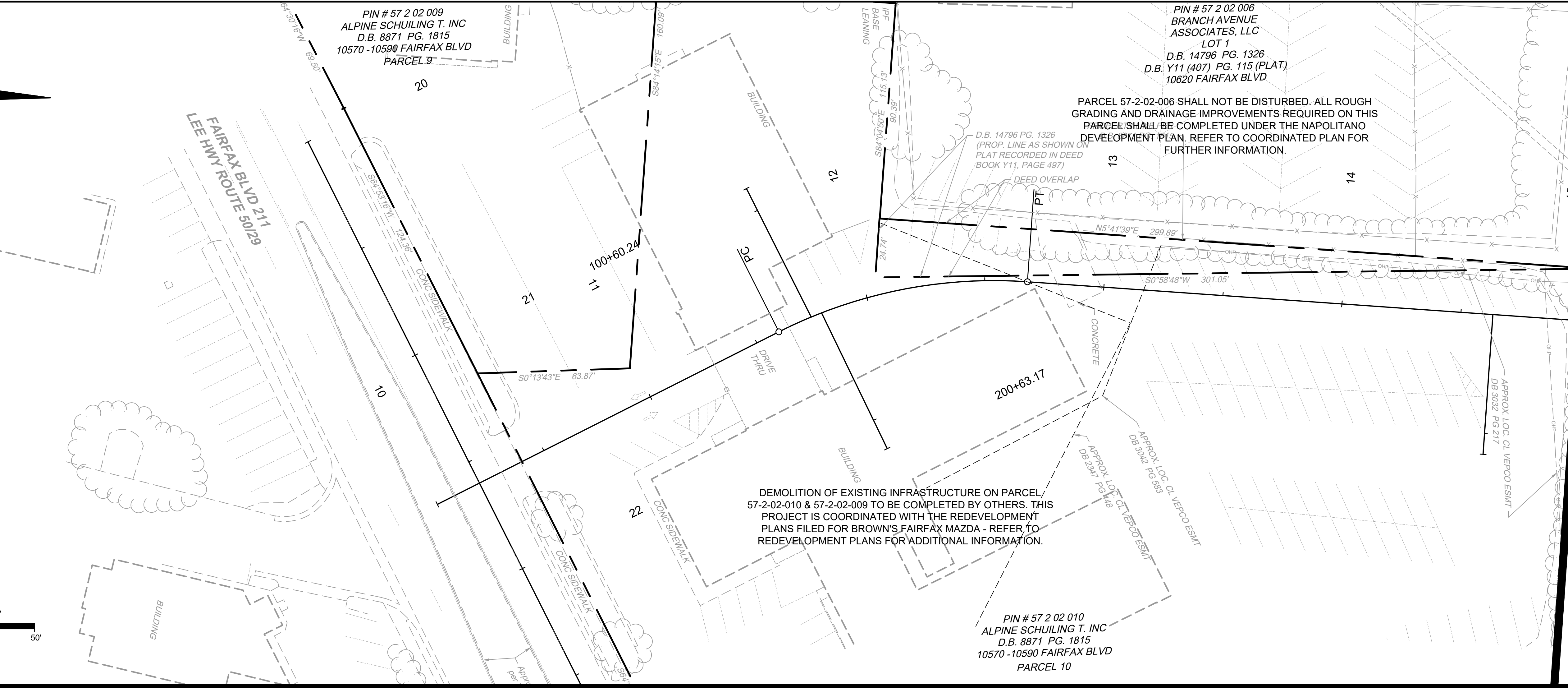
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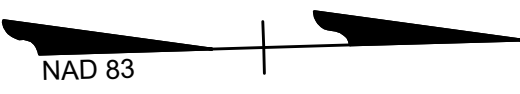
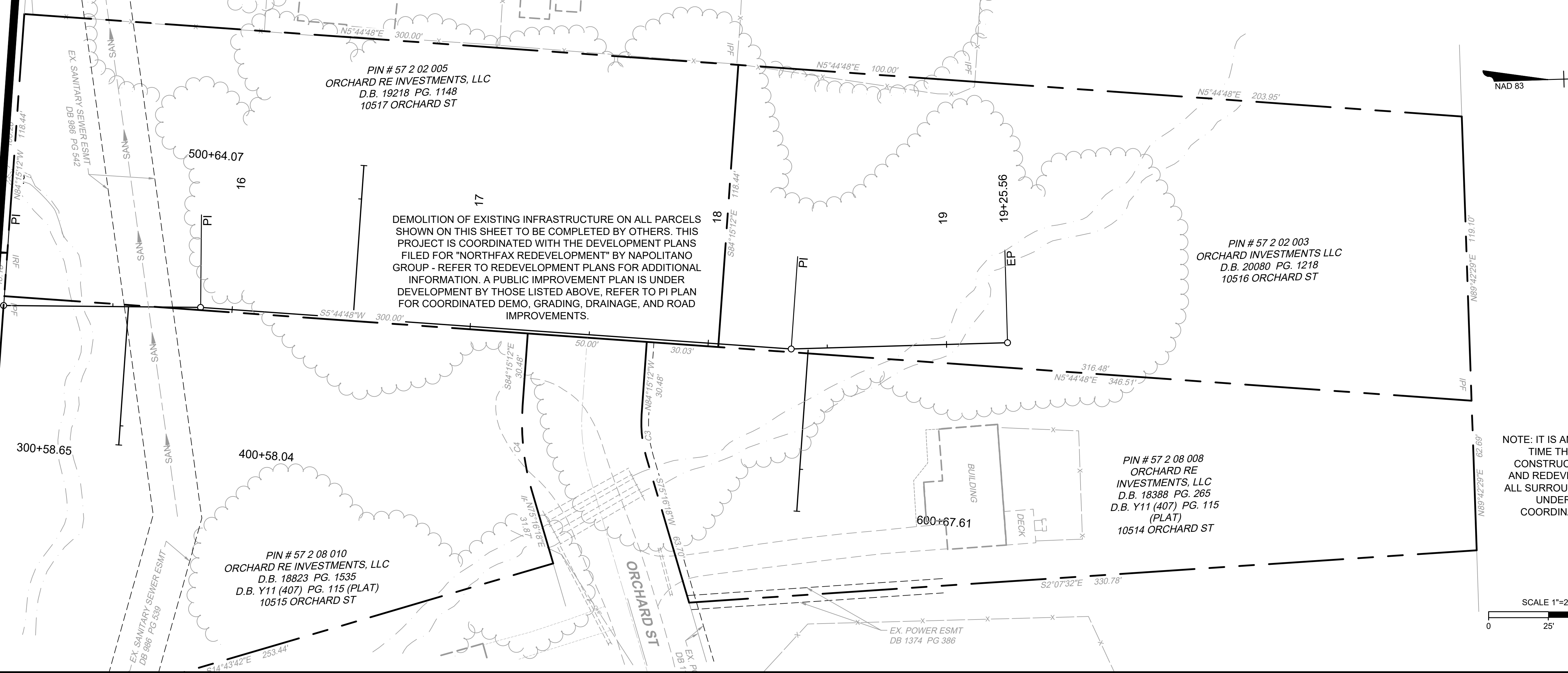


MATCHLINE STA. 15+00 - SEE ABOVE



MATCHLINE STA. 15+00 - SEE BELOW

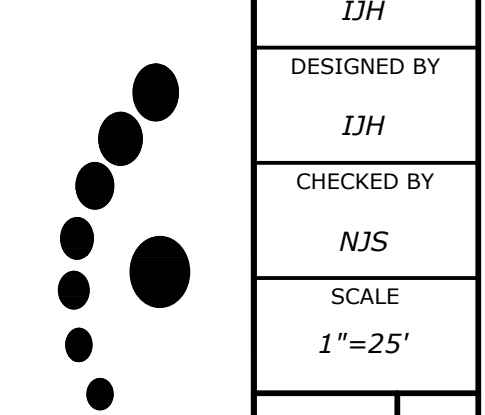
MATCHLINE STA. 15+00 - SEE ABOVE



NOTE: IT IS ANTICIPATED THAT AT THE TIME THIS PROJECT BEGINS CONSTRUCTION THE DEMOLITION AND REDEVELOPMENT EFFORTS OF ALL SURROUNDING PARCELS WILL BE UNDERWAY. ADDITIONAL COORDINATION DETAILS TO BE PROVIDED.

PLANS SUBJECT TO CHANGE
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YOUR VISION ACHIEVED THROUGH OURS.	Site Development	Residential	Infrastructure	Technology
	DATE	REVISION DESCRIPTION		
	DATE	04/28/2020		
	DRAWN BY	IJH		
	DESIGNED BY	IJH		
	CHECKED BY	NJS		
	SCALE	1"=25'		



TIMMONS GROUP

FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA

EXISTING CONDITIONS & DEMOLITION PLAN

JOB NO. 41013.009
SHEET NO. 01B

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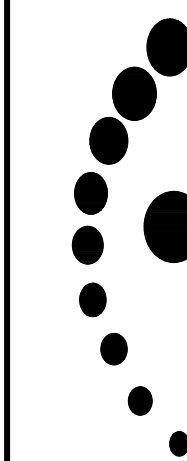
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DESIGNED BY: DJH
CHECKED BY: NJS
SCALE: 1"=25'



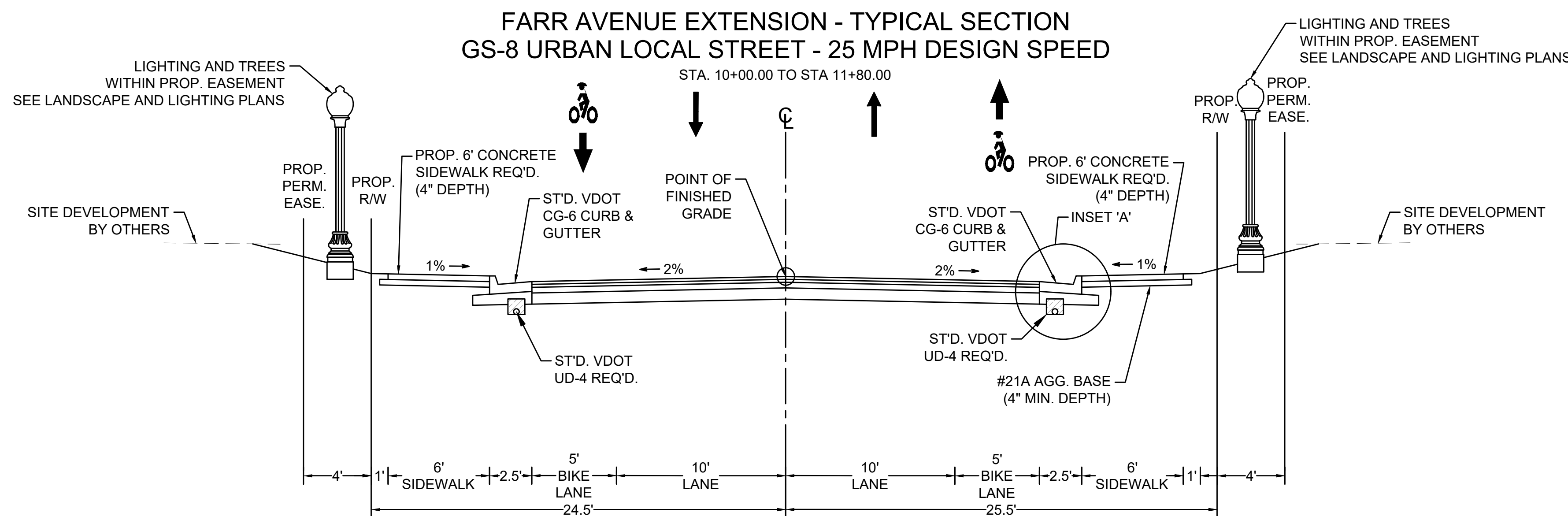
FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA

TYPICAL SECTIONS

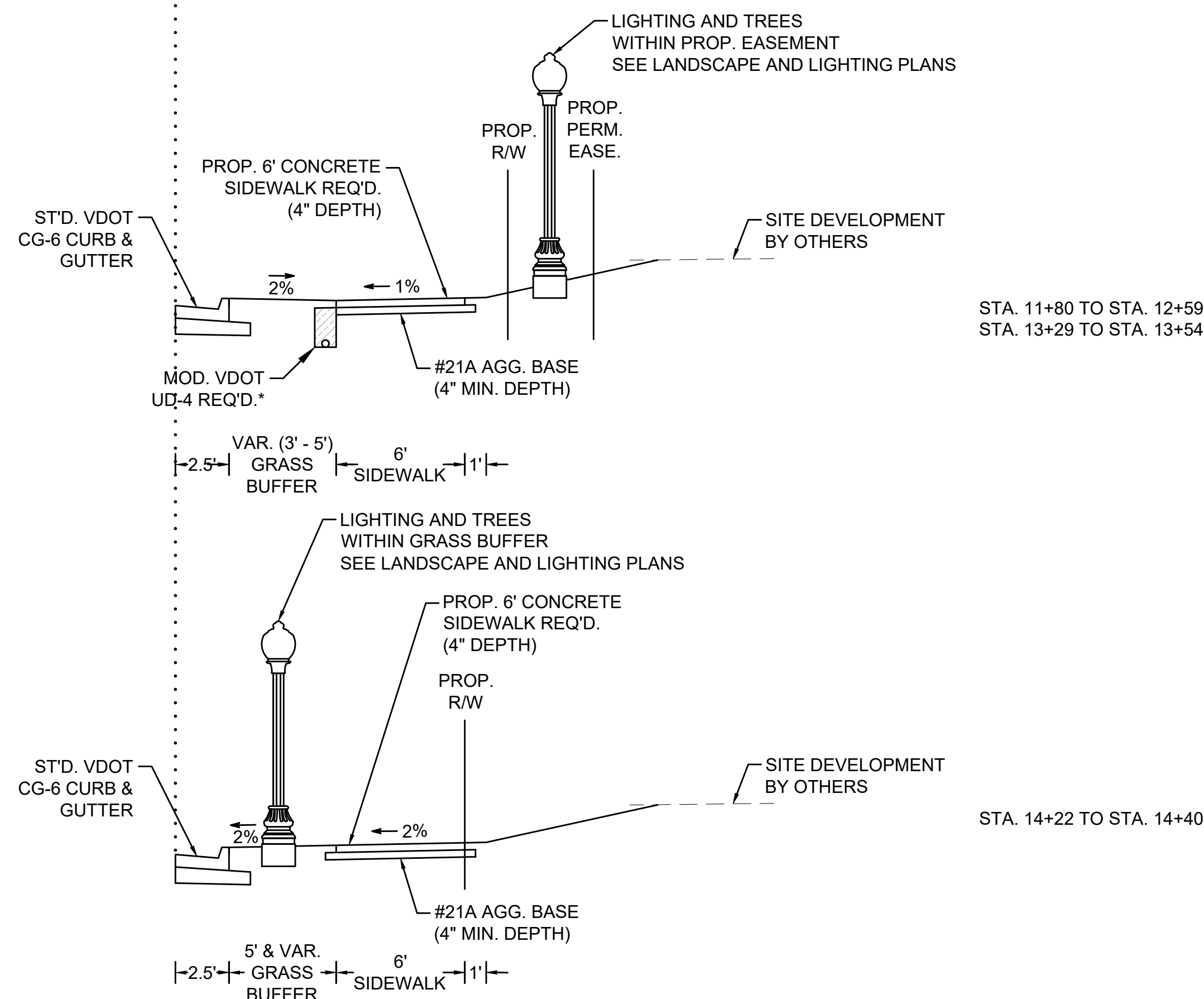
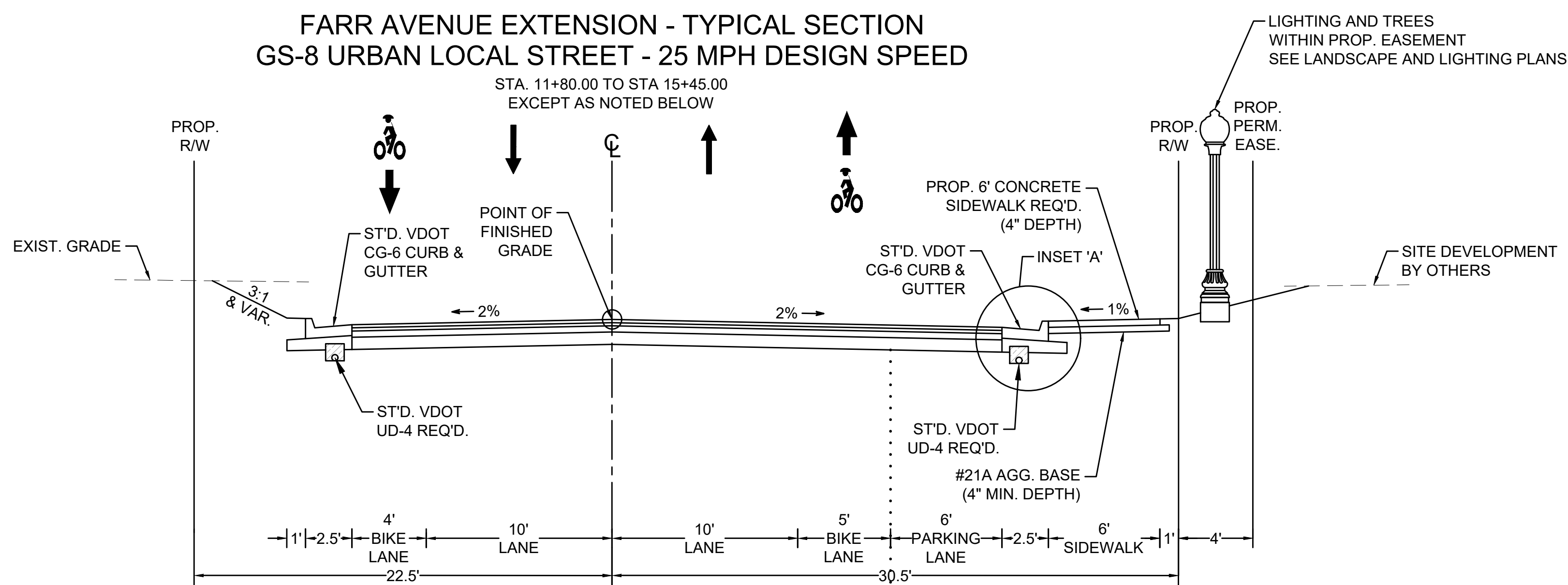
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SHEET NO. 02(1)

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FARR AVENUE EXTENSION - TYPICAL SECTION
GS-8 URBAN LOCAL STREET - 25 MPH DESIGN SPEED



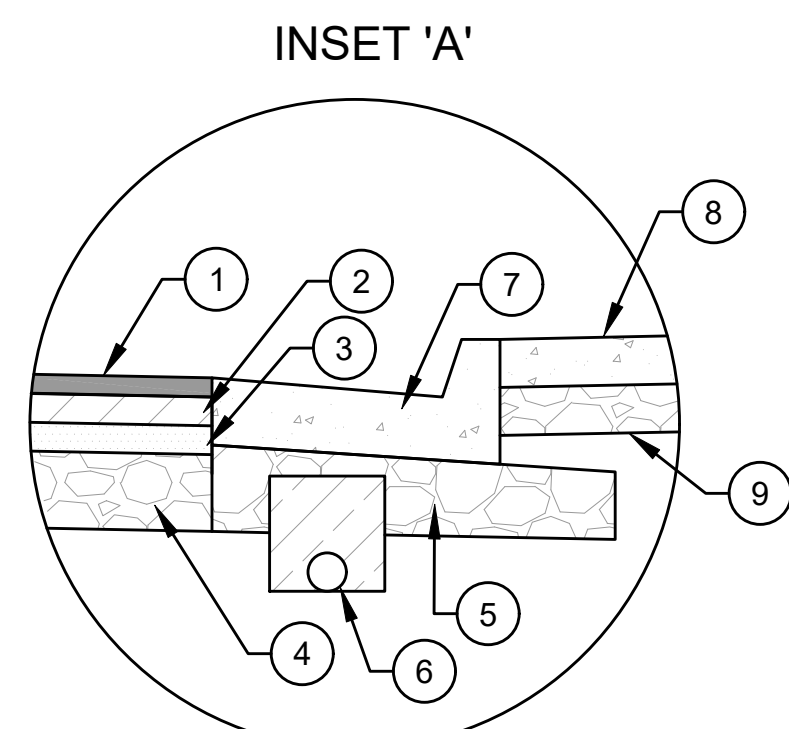
FARR AVENUE EXTENSION - TYPICAL SECTION
GS-8 URBAN LOCAL STREET - 25 MPH DESIGN SPEED



STA. 11+80 TO STA. 12+59
STA. 13+29 TO STA. 13+54

STA. 14+22 TO STA. 14+40

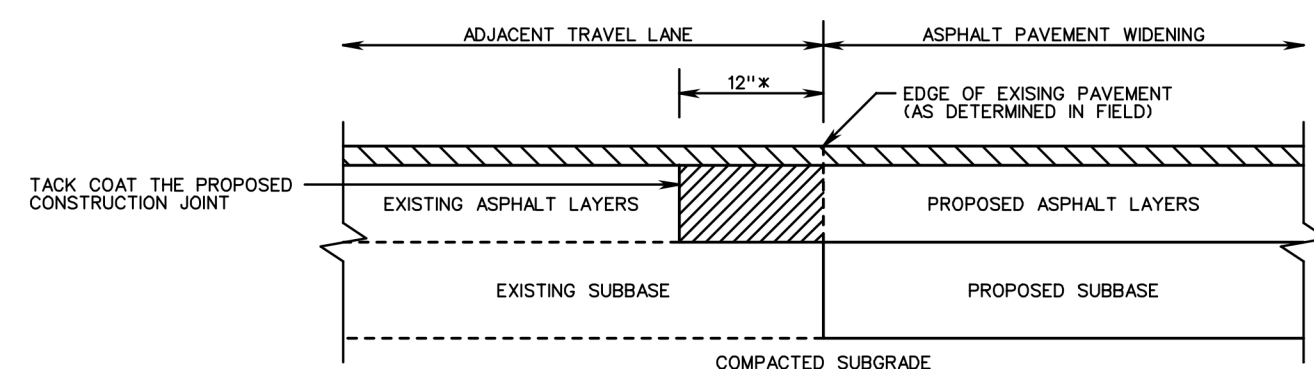
NOTES:
* UNDERDRAIN SHALL BE CONSTRUCTED AT A LOCATION AND DEPTH TO PROVIDE POSITIVE DRAINAGE OF PERFORATED PIPE THROUGH CURB BUMP-OUT AREAS. STONE SUBBASE OF SIDEWALK SHALL TIE TO STONE TRENCH FOR UNDERDRAIN.



NOT TO SCALE

- 1 2" ASPHALT CONCRETE SURFACE COURSE @ 220 LBS/SY (SM-9.5A)
- 2 2" ASPHALT CONCRETE INTERMEDIATE COURSE (IM-19.0)
- 3 2.5" ASPHALT CONCRETE BASE COURSE (BM-25.0)
- 4 8" AGGREGATE BASE MATERIAL (TYPE 1 NO. 21A)
- 5 VAR. DEPTH AGGREGATE BASE MATERIAL (VDOT TYPE 1, NO. 21A)
- 6 VDOT UD-4 UNDERDRAIN
- 7 VDOT CG-6 CURB AND GUTTER
- 8 4" HYDRAULIC CEMENT CONCRETE SIDEWALK
- 9 4" AGGREGATE BASE MATERIAL (TYPE 1 NO. 21A)

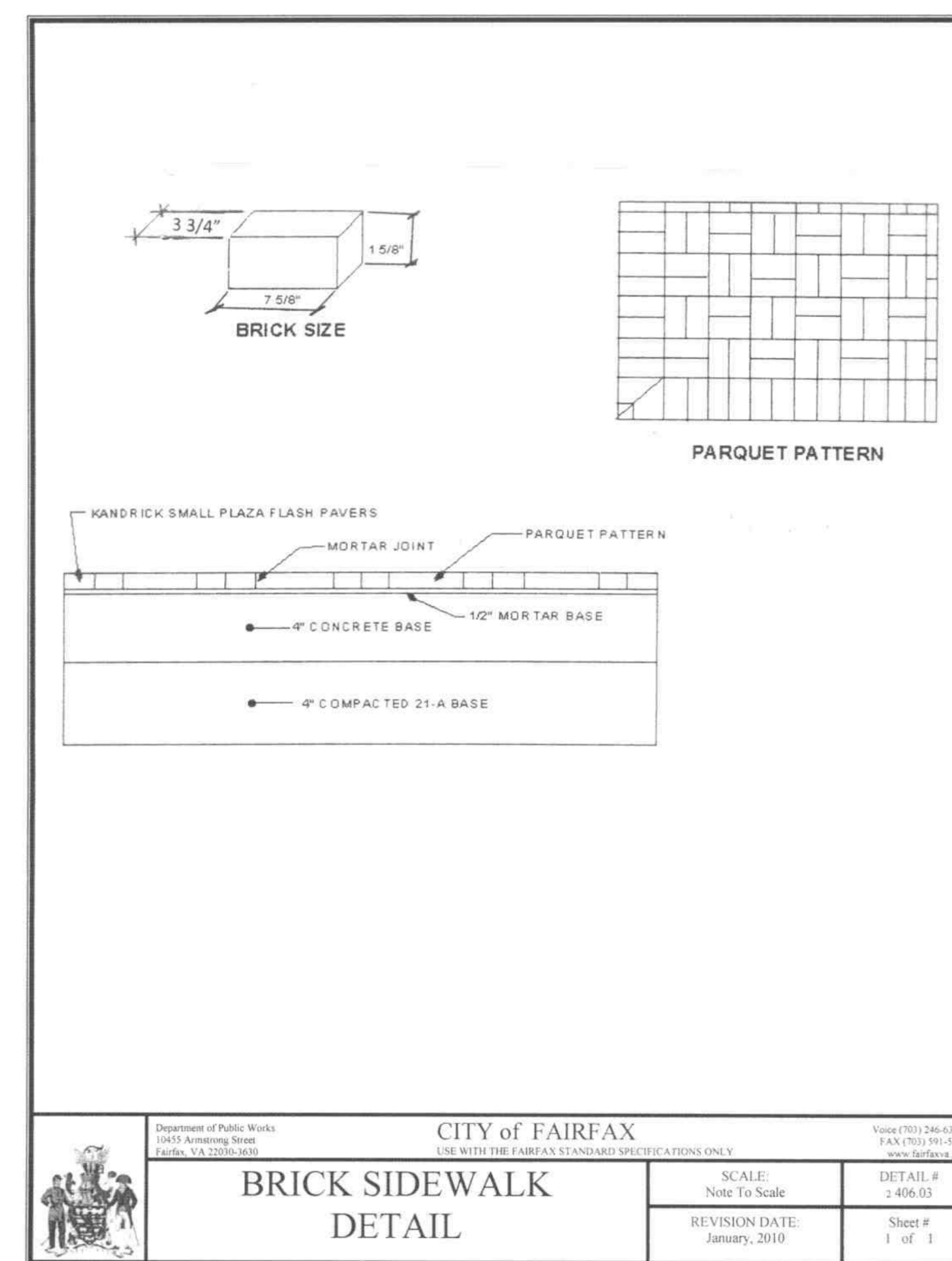
NOTE: CONNECTION BETWEEN FARR AVE. EXTENSION AND U.S. RTE. 29 SHALL BE ACHIEVED THROUGH THE USE OF VDOT ST'D. WP-2, SHOWN BELOW.



CONSTRUCTION JOINT DETAIL

- REMOVE EXISTING ASPHALT LAYERS TO EXISTING SUBBASE AND REPLACE WITH PROPOSED ASPHALT WIDENING LAYERS
- PROPOSED MINIMUM 1 1/2 INCH THICK ASPHALT SURFACE COURSE (SEE NOTE 5)
- MINIMUM 12 INCHES, OR GREATER AS NECESSARY TO ABUT THE FULL THICKNESS OF EXISTING ASPHALT LAYERS AS DETERMINED BY CORES (SEE NOTE 3)

- NOTES:
- ASPHALT PAVEMENT WIDENING SHALL HAVE A PAVEMENT DESIGN IN ACCORDANCE WITH CURRENT VDOT PROCEDURES AND BE APPROVED BY THE ENGINEER.
 - THE PAVEMENT DESIGN FOR ASPHALT PAVEMENT WIDENING SHALL MEET OR EXCEED THE DEPTHS AND TYPES OF THE LAYERS OF EXISTING PAVEMENT. SUBSURFACE DRAINAGE OF THE EXISTING AND PROPOSED PAVEMENT SHALL BE ADDRESSED IN THE PAVEMENT DESIGN.
 - A MINIMUM OF THREE CORES SHALL BE TAKEN ALONG THE CENTER OF THE ADJACENT TRAVEL LANE TO DETERMINE THE TYPE AND THICKNESS OF EXISTING PAVEMENT LAYERS. THESE CORES SHALL BE SPACED NO MORE THAN 500 FEET APART.
 - THE ADJACENT TRAVEL LANE SHALL BE MILLED A MINIMUM DEPTH OF 1 1/2 INCHES AND REPLACED WITH AN ASPHALT SURFACE COURSE TO MATCH THE PROPOSED PAVEMENT WIDENING SURFACE COURSE, UNLESS WAIVED BY THE ENGINEER.
 - THE ENGINEER MAY REQUIRE THE MILLING DEPTH OF THE EXISTING PAVEMENT TO BE ADJUSTED TO ACHIEVE AN ACCEPTABLE PAVEMENT CROSS-SLOPE AND EFFECTIVE SURFACE DRAINAGE.
 - EXISTING PAVEMENT MARKINGS AND MARKERS WITHIN THE PROJECT LIMITS SHALL BE RESTORED SUBJECT TO THE APPROVAL OF THE ENGINEER.
 - FINAL TRANSVERSE PAVEMENT TIE-IN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 315.05(c) OF THE SPECIFICATIONS EXCEPT THAT ALL JOINTS AT TIE-IN LOCATIONS SHALL BE TESTED USING A 10 FOOT STRAIGHTEDGE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 315.07(a) OF THE SPECIFICATIONS.



BRICK SIDEWALK DETAIL

Department of Public Works 2015 Approved Items	CITY OF FAIRFAX SOLID WASTEWATER TREATMENT PLANT & WINDS ONLY	Scale: Not To Scale Revision Date: January, 2010	Sheet # 1 of 3
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EROSION & SEDIMENT CONTROL NOTES

PLANS SUBJECT TO CHANGE

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PROJECT DESCRIPTION

THE PURPOSE OF THIS PROJECT IS TO CONSTRUCT A NEW NORTH-SOUTH ROADWAY BETWEEN FAIRFAX BLVD./FARR AVE. AND ORCHARD STREET TO CREATE A GRID NETWORK WITHIN THE NORTHWEST QUADRANT OF FAIRFAX BLVD./CHAIN BRIDGE RD. (NORTHFAV). BIKE LANES, SIDEWALKS, AND PARKING LANES ARE PROPOSED TO BE CONSTRUCTED TO CONNECT EXISTING FACILITIES. THE ROADWAY IMPROVEMENTS WILL REQUIRE ACQUISITION OF RIGHT OF WAY AND PERMANENT AND TEMPORARY EASEMENTS. THE TOTAL LAND DISTURBANCE FOR THIS PROJECT IS APPROXIMATELY 1.49 ACRES.

EXISTING SITE CONDITIONS:

THE SURROUNDING TOPOGRAPHY FOR THE FARR AVE. EXTENSION PROJECT CONSISTS OF EXISTING PARKING LOTS AND COMMERCIAL ESTABLISHMENTS, WHICH WILL BE DEMOLISHED PRIOR TO CONSTRUCTION OF FARR AVENUE EXTENSION. NORTHERN PORTIONS OF THE PROPOSED PROJECT AREA ARE POCKETS OF WOODED AREAS AND RESIDENTIAL PROPERTIES. THE ENTIRE SURROUNDING AREA IS INTENDED TO BE RE-DEVELOPED ALONGSIDE THE CONSTRUCTION OF FARR AVENUE EXTENSION.

ADJACENT PROPERTY:

THE SURROUNDING PROPERTIES OF THE PROJECT SITE CONSIST OF VARIOUS COMMERCIAL AND RESIDENTIAL PROPERTIES, INCLUDING A CAR DEALERSHIP.

SOILS:

THE PREDOMINANT SOIL FOR THIS PROJECT IS 95 - URBAN LAND. TOWARDS THE NORTHERN EXTENTS OF THE PROJECT, THE SOIL TYPE TRANSITIONS TO PRIMARILY 103A - WHEATON-CODORUS COMPLEX, 0 TO 2 PERCENT SLOPES, WITH SMALLER AREAS OF 105B - WHEATON-CLENELG COMPLEX, 2 TO 7 PERCENT SLOPES TO THE WEST OF THE ROAD. CONTRACTOR IS TO TAKE CAUTION IF ASBESTOS IS FOUND IN THE SOIL. SEE ENVIRONMENTAL NOTE #4 ON SHEET 2A FOR MORE DETAILS.

OFF-SITE AREAS:

NO OFF-SITE TOPSOIL STOCKPILE AREAS ARE NEEDED FOR THIS PROJECT. TOPSOIL STOCKPILES WILL BE LOCATED AND DETERMINED BY THE CONTRACTOR.

CRITICAL EROSION AREAS:

CRITICAL AREAS ARE AS FOLLOWS:

- ALL ADJACENT AND CONNECTING ROADS AND ENTRANCES FOR THE LENGTH OF THE PROJECT. CONTRACTOR MUST KEEP THE EXISTING ROAD FROM BUILD-UP OF SOIL.
- ADJACENT COMMERCIAL PROPERTIES
- SLOPES STEEPER THAN 3:1 SHALL BE PROTECTED WITH EC-2 MATTING TO PREVENT EROSION

EROSION AND SEDIMENT CONTROL MEASURES:

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED ACCORDING TO THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE HANDBOOK. THE MINIMUM STANDARDS OF THE VESCH SHALL BE ADHERED TO UNLESS OTHERWISE WAIVED OR APPROVED BY A VARIANCE.

EROSION & SEDIMENT CONTROL SEQUENCE OF CONSTRUCTION:

- SCHEDULE A PRECONSTRUCTION MEETING, GIVE A 48-HOUR NOTIFICATION OF THE PRECONSTRUCTION MEETING AND A CERTIFIED RESPONSIBLE LAND DISTURBER (CRLD) MUST BE PRESENT AT THE MEETING. VSMP OPERATIVE SHALL ATTEND THE PRECONSTRUCTION MEETING.
- PROVIDE MINIMAL CLEARING TO INSTALL SILT FENCE AND SAFETY FENCE AS SHOWN ON THE PLANS. INSTALL INLET PROTECTION AS REQUIRED FOR EXISTING INLETS. TAKE SPECIAL CARE TO KEEP SOIL BUILD-UP FROM ENTERING THE EXISTING ROAD PAVEMENT.
- THE CONTRACTOR WILL NOTIFY THE CITY INSPECTOR AT THE PRE-CONSTRUCTION MEETING OF THE PERMITTED LOCATION EXCESS SOIL MATERIAL IS HAULED TO OR BORROW MATERIAL IS BROUGHT IN FROM. CLEAR AND GRUB AS INDICATED IN THE PLANS. MAINTAIN ACCESS ALONG EXISTING ROADS AT ALL TIMES.
- TEMPORARY GRADING AND SEEDING IS REQUIRED WITHIN 7 DAYS OF DISTURBANCE FOR ALL AREAS WHICH ARE NOT TO BE ACTIVELY CONSTRUCTED UPON WITHIN 14 DAYS OF INITIAL DISTURBANCE.
- INSPECTIONS AND APPROVALS FOR COMPLETION OF INITIAL EROSION AND SEDIMENT CONTROL ACTIVITIES MUST BE RECEIVED BEFORE FURTHER CONSTRUCTION ACTIVITIES CAN BEGIN. ALL APPROVALS SHALL BE FROM THE CITY OF FAIRFAX.
- COMMENCE ROUGH GRADING AS REQUIRED FOR PROPOSED PAVEMENT WIDENING.
- THE SITE SHALL BE PERMANENTLY STABILIZED AFTER ALL GRADING HAS BEEN COMPLETED BY SEEDING ALL DENUDED AREAS.
- INSTALL PROPOSED DRAINAGE STRUCTURES AND INSTALL INLET PROTECTION FOR PROPOSED FACILITIES AS DENOTED ON THE PLANS. MAINTAIN SILT FENCE AND SAFETY FENCE AT ALL TIMES AS DENOTED ON THE PLANS.
- INSTALL STONE BASE FOR ROAD WIDENING AND ANY INTERMEDIATE AND/OR TOP COURSES OF ASPHALT ONCE FINAL GRADES ARE REACHED.
- REFER TO PLAN SHEET XX FOR DETAILS REGARDING MAINTENANCE OF TRAFFIC.
- UPON CONSTRUCTION COMPLETION, THE CONTRACTOR MUST CONTACT THE CITY OF FAIRFAX FOR EROSION CONTROL INSPECTION OF SLOPE STABILITY. EROSION CONTROL MEASURES MAY NOT BE REMOVED WITHOUT AUTHORIZATION BY THE CITY INSPECTOR.
- ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUESTED BY THE CITY OF FAIRFAX AND/OR THE INSPECTOR AT ANY TIME DURING LAND DISTURBANCE.

MINIMUM STANDARDS

AN EROSION AND SEDIMENT CONTROL PROGRAM ADOPTED BY A DISTRICT OR LOCALITY MUST BE CONSISTENT WITH THE FOLLOWING CRITERIA, TECHNIQUES AND METHODS:

- MS-1. PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED WITHIN SEVEN DAYS TO DENUDED AREAS THAT MAY NOT BE AT FINAL GRADE BUT WILL REMAIN DORMANT FOR LONGER THAN 14 DAYS. PERMANENT STABILIZATION SHALL BE APPLIED TO AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN ONE YEAR.
- MS-2. DURING CONSTRUCTION OF THE PROJECT, SOIL STOCKPILES AND BORROW AREAS SHALL BE STABILIZED OR PROTECTED WITH SEDIMENT TRAPPING MEASURES. THE APPLICANT IS RESPONSIBLE FOR THE TEMPORARY PROTECTION AND PERMANENT STABILIZATION OF ALL SOIL STOCKPILES ON SITE AS WELL AS BORROW AREAS AND SOIL INTENTIONALLY TRANSPORTED FROM THE PROJECT SITE.
- MS-3. A PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED ON DENUDED AREAS NOT OTHERWISE PERMANENTLY STABILIZED. PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL A GROUND COVER IS ACHIEVED THAT IS UNIFORM, MATURE ENOUGH TO SURVIVE AND WILL INHIBIT EROSION.
- MS-4. SEDIMENT BASINS AND TRAPS, PERIMETER DIKES, SEDIMENT BARRIERS AND OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND-DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UPSLOPE LAND DISTURBANCE TAKES PLACE.
- MS-5. STABILIZATION MEASURES SHALL BE APPLIED TO EARTHEN STRUCTURES SUCH AS DAMS, DIKES AND DIVERSIONS IMMEDIATELY AFTER INSTALLATION.
- MS-6. SEDIMENT TRAPS AND SEDIMENT BASINS SHALL BE DESIGNED AND CONSTRUCTED BASED UPON THE TOTAL DRAINAGE AREA TO BE SERVED BY THE TRAP OR BASIN.

A. THE MINIMUM STORAGE CAPACITY OF A SEDIMENT TRAP SHALL BE 134 CUBIC YARDS PER ACRE OF DRAINAGE AREA AND THE TRAP SHALL ONLY CONTROL DRAINAGE AREAS LESS THAN THREE ACRES.

B. SURFACE RUNOFF FROM DISTURBED AREAS THAT IS COMPRISED OF FLOW FROM DRAINAGE AREAS GREATER THAN OR EQUAL TO THREE ACRES SHALL BE CONTROLLED BY A SEDIMENT BASIN. THE MINIMUM STORAGE CAPACITY OF A SEDIMENT BASIN SHALL BE 134 CUBIC YARDS PER ACRE OF DRAINAGE AREA. THE OUTFALL SYSTEM SHALL, AT A MINIMUM, MAINTAIN THE STRUCTURAL INTEGRITY OF THE BASIN DURING A 25-YEAR STORM OF 24-HOUR DURATION. RUNOFF COEFFICIENTS USED IN RUNOFF CALCULATIONS SHALL CORRESPOND TO A BARE EARTH CONDITION OR THOSE CONDITIONS EXPECTED TO EXIST WHILE THE SEDIMENT BASIN IS UTILIZED.

- MS-7. CUT AND FILL SLOPES SHALL BE DESIGNED AND CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. SLOPES THAT ARE FOUND TO BE ERODING EXCESSIVELY WITHIN ONE YEAR OF PERMANENT STABILIZATION SHALL BE PROVIDED WITH ADDITIONAL SLOPE STABILIZING MEASURES UNTIL THE PROBLEM IS CORRECTED.
- MS-8. CONCENTRATED RUNOFF SHALL NOT FLOW DOWN CUT OR FILL SLOPES UNLESS CONTAINED WITHIN AN ADEQUATE TEMPORARY OR PERMANENT CHANNEL, FLUME OR SLOPE DRAIN STRUCTURE.
- MS-9. WHENEVER WATER SEEPS FROM A SLOPE FACE, ADEQUATE DRAINAGE OR OTHER PROTECTION SHALL BE PROVIDED.
- MS-10. ALL STORM SEWER INLETS THAT ARE MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT SEDIMENT-LADEN WATER CANNOT ENTER THE CONVEYANCE SYSTEM WITHOUT FIRST BEING FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT.
- MS-11. BEFORE NEWLY CONSTRUCTED STORMWATER CONVEYANCE CHANNELS OR PIPES ARE MADE OPERATIONAL, ADEQUATE OUTLET PROTECTION AND ANY REQUIRED TEMPORARY OR PERMANENT CHANNEL LINING SHALL BE INSTALLED IN BOTH THE CONVEYANCE CHANNEL AND RECEIVING CHANNEL.
- MS-12. WHEN WORK IN A LIVE WATERCOURSE IS PERFORMED, PRECAUTIONS SHALL BE TAKEN TO MINIMIZE ENCROACHMENT, CONTROL SEDIMENT TRANSPORT AND STABILIZE THE WORK AREA TO THE GREATEST EXTENT POSSIBLE DURING CONSTRUCTION. NONERODIBLE MATERIAL SHALL BE USED FOR THE CONSTRUCTION OF CAUSEWAYS AND COFFERDAMS. EARTHEN FILL MAY BE USED FOR THESE STRUCTURES IF ARMORED BY NONERODIBLE COVER MATERIALS.
- MS-13. WHEN A LIVE WATERCOURSE MUST BE CROSSED BY CONSTRUCTION VEHICLES MORE THAN TWICE IN ANY SIX-MONTH PERIOD, A TEMPORARY VEHICULAR STREAM CROSSING CONSTRUCTED OF NONERODIBLE MATERIAL SHALL BE PROVIDED.
- MS-14. ALL APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS PERTAINING TO WORKING IN OR CROSSING LIVE WATERCOURSES SHALL BE MET.
- MS-15. THE BED AND BANKS OF A WATERCOURSE SHALL BE STABILIZED IMMEDIATELY AFTER WORK IN THE WATERCOURSE IS COMPLETED.
- MS-16. UNDERGROUND UTILITY LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE FOLLOWING STANDARDS IN ADDITION TO OTHER APPLICABLE CRITERIA:
- NO MORE THAN 500 LINEAR FEET OF TRENCH MAY BE OPENED AT ONE TIME.
 - EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF TRENCHES.
 - EFFLUENT FROM DEWATERING OPERATIONS SHALL BE FILTERED OR PASSED THROUGH AN APPROVED SEDIMENT TRAPPING DEVICE, OR BOTH, AND DISCHARGED IN A MANNER THAT DOES NOT ADVERSELY AFFECT FLOWING STREAMS OR OFF-SITE PROPERTY.
 - MATERIAL USED FOR BACKFILLING TRENCHES SHALL BE PROPERLY COMPACTED IN ORDER TO MINIMIZE EROSION AND PROMOTE STABILIZATION.
 - RE-STABILIZATION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THESE REGULATIONS.
 - APPLICABLE SAFETY REGULATIONS SHALL BE COMPLIED WITH.

MINIMUM STANDARDS (continued)

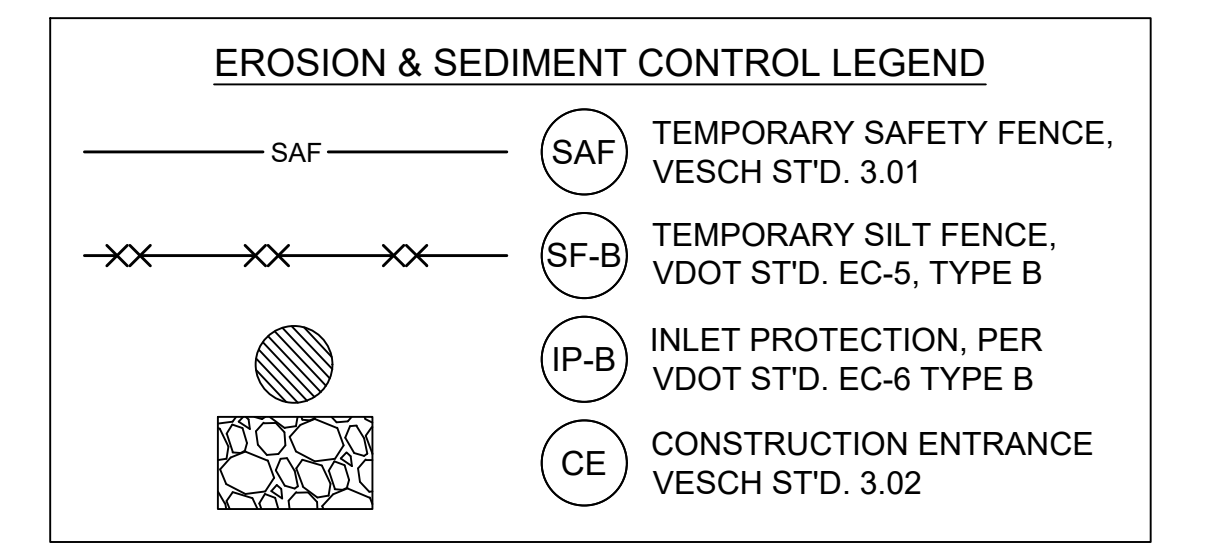
- MS-17. WHERE CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED OR PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY VEHICULAR TRACKING ONTO THE PAVED SURFACE. WHERE SEDIMENT IS TRANSPORTED ONTO A PAVED OR PUBLIC ROAD SURFACE, THE ROAD SURFACE SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM THE ROADS BY SHOVELING OR SWEEPING AND TRANSPORTED TO A SEDIMENT CONTROL DISPOSAL AREA. STREET WASHING SHALL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER. THIS PROVISION SHALL APPLY TO INDIVIDUAL DEVELOPMENT LOTS AS WELL AS TO LARGER LAND-DISTURBING ACTIVITIES.
- MS-18. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, UNLESS OTHERWISE AUTHORIZED BY THE LOCAL PROGRAM AUTHORITY. TRAPPED SEDIMENT AND THE DISTURBED SOIL AREAS RESULTING FROM THE DISPOSITION OF TEMPORARY MEASURES SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION AND SEDIMENTATION.
- MS-19. PROPERTIES AND WATERWAYS DOWNSTREAM FROM DEVELOPMENT SITES SHALL BE PROTECTED FROM SEDIMENT DEPOSITION, EROSION AND DAMAGE DUE TO INCREASES IN VOLUME, VELOCITY AND PEAK FLOW RATE OF STORMWATER RUNOFF FOR THE STATED FREQUENCY STORM OF 24-HOUR DURATION IN ACCORDANCE WITH THE FOLLOWING STANDARDS AND CRITERIA:
- CONCENTRATED STORMWATER RUNOFF LEAVING A DEVELOPMENT SITE SHALL BE DISCHARGED DIRECTLY INTO AN ADEQUATE NATURAL OR MAN-MADE RECEIVING CHANNEL, PIPE OR STORM SEWER SYSTEM. FOR THOSE SITES WHERE RUNOFF IS DISCHARGED INTO A PIPE OR PIPE SYSTEM, DOWNSTREAM STABILITY ANALYSIS AT THE OUTFALL OF THE PIPE OR PIPE SYSTEM SHALL BE PERFORMED.
 - ADEQUACY OF ALL CHANNELS AND PIPES SHALL BE VERIFIED IN THE FOLLOWING MANNER:
 - THE APPLICANT SHALL DEMONSTRATE THAT THE TOTAL DRAINAGE AREA TO THE POINT OF ANALYSIS WITHIN THE CHANNEL IS ONE HUNDRED TIMES GREATER THAN THE CONTRIBUTING DRAINAGE AREA OF THE PROJECT IN QUESTION.
 - NATURAL CHANNELS SHALL BE ANALYZED BY THE USE OF A TWO-YEAR FREQUENCY STORM TO VERIFY THAT THE STORMWATER WILL NOT OVERTOP CHANNEL BANKS NOR CAUSE EROSION OF CHANNEL BED OR BANKS.
 - ALL PREVIOUSLY CONSTRUCTED MAN-MADE CHANNELS SHALL BE ANALYZED BY THE USE OF A TEN-YEAR FREQUENCY STORM TO VERIFY THAT STORMWATER WILL NOT OVERTOP ITS BANKS AND BY THE USE OF A TWO-YEAR STORM TO DEMONSTRATE THAT STORMWATER WILL BE CONTAINED WITHIN THE PIPE OR SYSTEM.
 - PIPES AND STORM SEWER SYSTEMS SHALL BE ANALYZED BY THE USE OF A TEN-YEAR FREQUENCY STORM TO VERIFY THAT THE STORMWATER WILL BE CONTAINED WITHIN THE PIPE OR SYSTEM.
 - IF EXISTING NATURAL RECEIVING CHANNELS OR PREVIOUSLY CONSTRUCTED MAN-MADE CHANNELS OR PIPES ARE NOT ADEQUATE, THE APPLICANT SHALL:
 - IMPROVE THE CHANNELS TO A CONDITION WHERE A TEN-YEAR FREQUENCY STORM WILL NOT OVERTOP THE BANKS AND A TWO-YEAR FREQUENCY STORM WILL NOT CAUSE EROSION TO THE CHANNEL, BED OR BANKS; OR
 - IMPROVE THE PIPE OR PIPE SYSTEM TO A CONDITION WHERE THE TEN-YEAR FREQUENCY STORM IS CONTAINED WITHIN THE APPURTENANCES; OR
 - DEVELOP A SITE DESIGN THAT WILL NOT CAUSE THE PRE-DEVELOPMENT PEAK RUNOFF RATE FROM A TWO-YEAR STORM TO INCREASE WHEN RUNOFF OUTFALLS INTO A NATURAL CHANNEL OR WILL NOT CAUSE THE PRE-DEVELOPMENT PEAK RUNOFF RATE FROM A TEN-YEAR STORM TO INCREASE WHEN RUNOFF OUTFALLS INTO A MAN-MADE CHANNEL.
 - PROVIDE A COMBINATION OF CHANNEL IMPROVEMENT, STORMWATER DETENTION/RETENTION OR OTHER MEASURES WHICH IS SATISFACTORY TO THE PLAN APPROVING AUTHORITY TO PREVENT DOWNSTREAM EROSION.
 - THE APPLICANT SHALL PROVIDE EVIDENCE OF PERMISSION TO MAKE THE IMPROVEMENTS.
 - ALL HYDROLOGICAL ANALYSIS SHALL BE BASED ON THE EXISTING WATERSHED CHARACTERISTICS AND THE ULTIMATE DEVELOPMENT CONDITION OF THE SUBJECT PROJECT.
 - IF THE APPLICANT CHOOSES AN OPTION THAT INCLUDES STORMWATER DETENTION/RETENTION, HE SHALL OBTAIN APPROVAL FROM THE LOCALITY OF A PLAN FOR MAINTENANCE OF THE DETENTION FACILITIES. THE PLAN SHALL SET FORTH THE MAINTENANCE REQUIREMENTS OF THE FACILITY AND THE PERSON RESPONSIBLE FOR PERFORMING THE MAINTENANCE.
 - INCREASED VOLUMES OF SHEET FLOWS THAT MAY CAUSE EROSION OR SEDIMENTATION ON ADJACENT PROPERTY SHALL BE DIVERTED TO A STABLE OUTLET, ADEQUATE CHANNEL OR DETENTION FACILITY.
 - IN APPLYING THESE STORMWATER MANAGEMENT CRITERIA, INDIVIDUAL LOTS IN A RESIDENTIAL SUBDIVISION DEVELOPMENT SHALL NOT BE CONSIDERED TO BE SEPARATE DEVELOPMENT PROJECTS. INSTEAD, THE RESIDENTIAL SUBDIVISION DEVELOPMENT, AS A WHOLE, SHALL BE CONSIDERED TO BE A SINGLE DEVELOPMENT PROJECT. HYDROLOGIC PARAMETERS THAT REFLECT THE ULTIMATE SUBDIVISION DEVELOPMENT SHALL BE USED IN ALL ENGINEERING CALCULATIONS.
 - PROPOSED COMMERCIAL OR INDUSTRIAL SUBDIVISIONS SHALL APPLY THESE STORMWATER MANAGEMENT CRITERIA TO THE DEVELOPMENT AS A WHOLE, HYDROLOGIC PARAMETERS THAT REFLECT THE ULTIMATE SUBDIVISION DEVELOPMENT SHALL BE USED IN ALL ENGINEERING CALCULATIONS.

ADDITIONAL NOTES:

- CONTRACTOR MUST KEEP ALL EXISTING ROADS FREE FROM THE BUILDUP OF SOIL, SEDIMENT, AND DEBRIS.
- SHOULD THE CONTRACTOR DETERMINE THAT A STOCKPILE AREA IS REQUIRED, THE LOCATION AND LIMITS OF THE STOCKPILE AREA MUST BE SELECTED BY THE CONTRACTOR. IF THE AREA IS OUTSIDE THE LIMITS OF DISTURBANCE AS SHOWN ON THESE PLANS, THE LOCATION MUST BE SUBMITTED TO AND APPROVED BY THE CITY OF FAIRFAX PRIOR TO CONSTRUCTION.
- THE CONTRACTOR WILL PROVIDE THE LOCATION THAT EXCESS SOIL IS HAULED TO, OR BORROW MATERIAL IS SOURCED FROM, TO THE ENVIRONMENTAL ENGINEERING INSPECTOR AT THE PRE-CONSTRUCTION MEETING.
- ANY ADDITIONAL PERMITTING FOR OFFSITE STOCKPILES, BORROW SOURCES, OR STAGING OF MEN/EQUIPMENT (IF REQUIRED) SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

EROSION & SEDIMENT CONTROL QUANTITIES

TEMPORARY SILT FENCE	1770 LF
TEMPORARY SAFETY FENCE	750 LF
LIMITS OF DISTURBANCE	1.49 AC
INLET PROTECTION TYPE B	12 EA
SILTATION CONTROL EXCAVATION	50 CY
TEMPORARY SEEDING	0.72 AC
PERMANENT SEEDING	0.72 AC



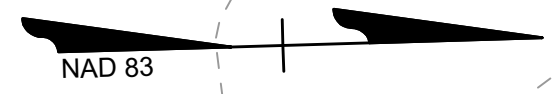
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TEL: 804.200.6500 FAX: 804.560.1016 www.timmons.com

YOUR VISION ACHIEVED THROUGH OURS.

DATE: 04/28/2020
DRAWN BY: JH
DESIGNED BY: JH
CHECKED BY: NJS
SCALE: 1"=25'

TIMMONS GROUP
FARR AVENUE EXT. (NORTHFAV WEST)
CITY OF FAIRFAX - VIRGINIA
EROSION & SEDIMENT CONTROL NOTES

JOB NO.
41013.009
SHEET NO.
02B

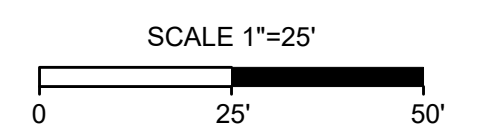


PIN # 57 2 02 009
ALPINE SCHULING T. INC
D.B. 8871 PG. 1815
10570 -10590 FAIRFAX BLVD
PARCEL 9

PIN # 57 2 02 006
BRANCH AVENUE
ASSOCIATES, LLC
LOT 1
D.B. 14796 PG. 1326
D.B. Y11 (407) PG. 115 (PLAT)
10620 FAIRFAX BLVD

PROPOSED GRADING AS SHOWN IS ANTICIPATED TO BE COMPLETED BY OTHERS PER NORTHFAX PUBLIC IMPROVEMENT PLAN (UNDER DEVELOPMENT). CONSTRUCTION LIMITS AND EROSION CONTROL MEASURES FOR THIS PROJECT TO BE UPDATED ACCORDINGLY.

NOTE: ALL PROPOSED GRADING SHOWN ON THIS SHEET IS CONSIDERED TEMPORARY AND SUBJECT TO CHANGE PENDING COMPLETION OF ADJACENT PARCEL DEVELOPMENT GRADING DETAIL PLAN. THIS PROJECT HAS BEEN DESIGNED TO MATCH ANTICIPATED SITE ELEVATIONS AT PROPOSED RIGHT OF WAY LINES.



MATCHLINE STA. 15+00 - SEE BELOW

PLANS SUBJECT TO CHANGE
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YOUR VISION ACHIEVED THROUGH OURS.	DATE	REVISION DESCRIPTION
	04/28/2020	
Site Development	Residential	Infrastructure
	Technology	

DESIGNED BY	IJH
CHECKED BY	NJS
SCALE	1"=25'

TIMMONS GROUP

FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA

EROSION & SEDIMENT CONTROL PLAN

JOB NO.	41013.009
SHEET NO.	02C

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Site Development Residential Infrastructure Technology

DATE	REVISION DESCRIPTION
04/28/2020	

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 DESIGNED BY: JH
 CHECKED BY: NJS
 SCALE: 1"=25'

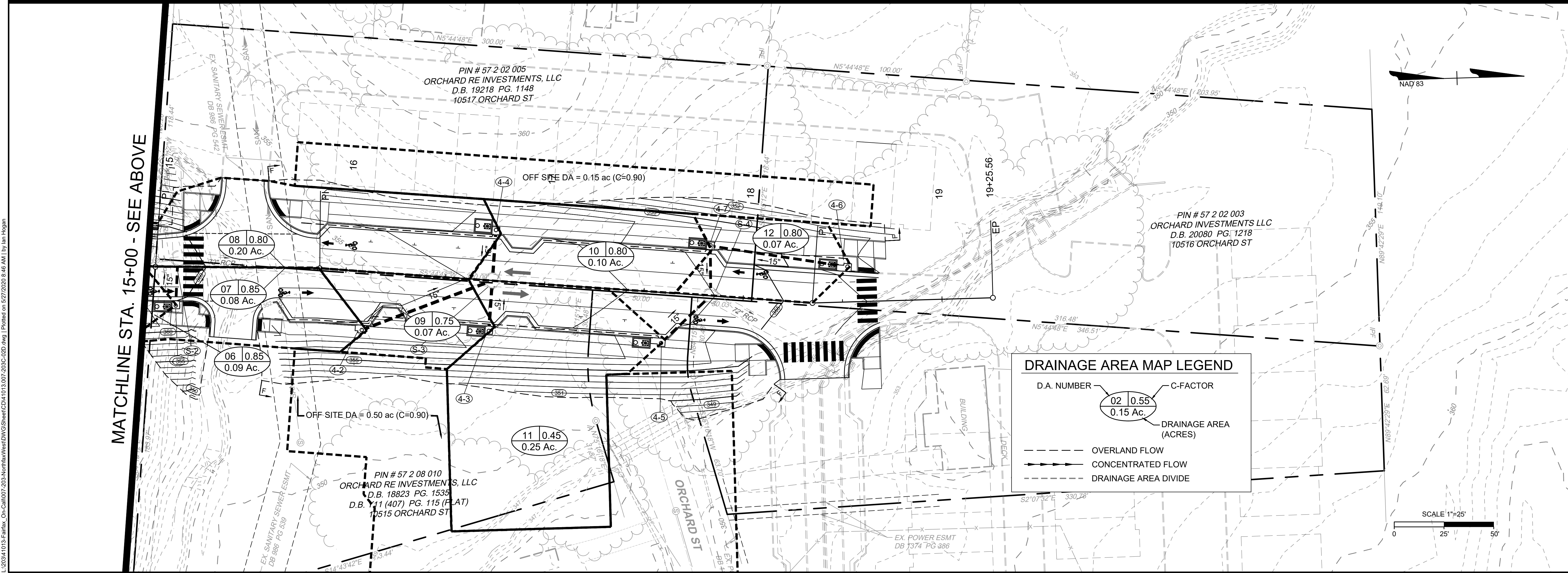
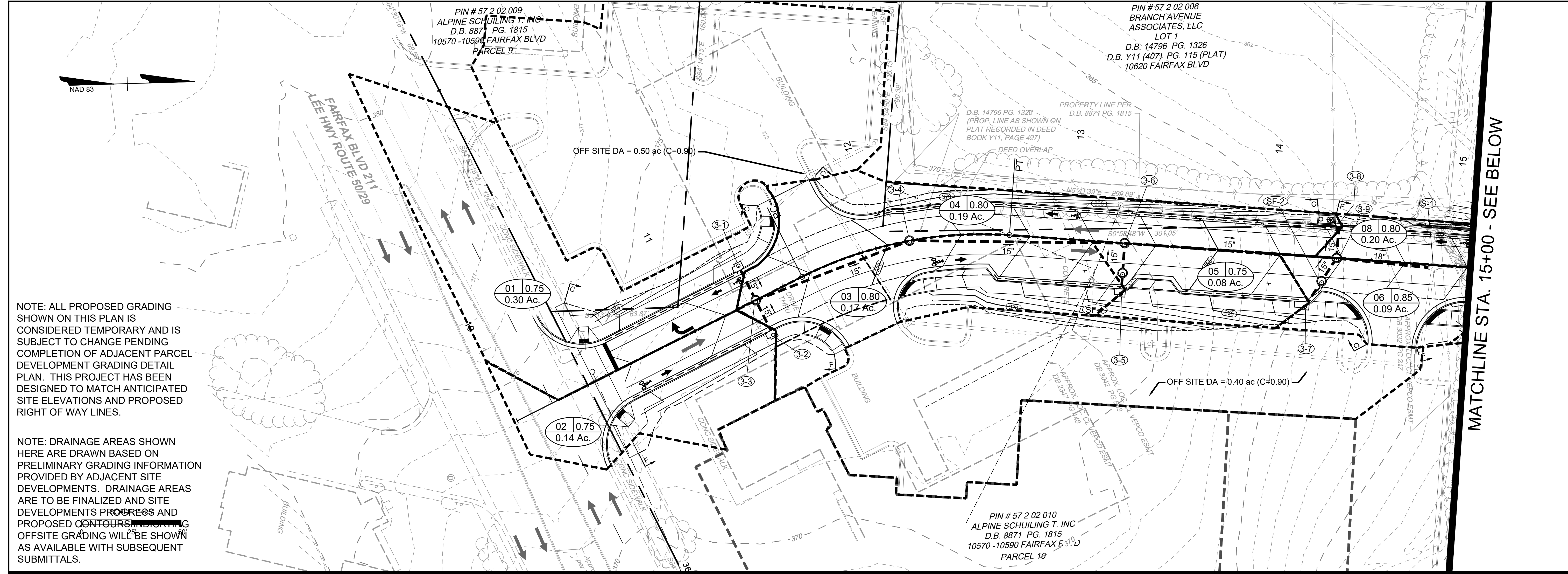
TIMMONS GROUP

FARR AVENUE EXT. (NORTHFAX WEST)
 CITY OF FAIRFAX - VIRGINIA

DRAINAGE AREA MAP

JOB NO.
41013.009
 SHEET NO.
02D

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ROADSIDE DEVELOPMENT

PLANS SUBJECT TO CHANGE
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CORE MIX

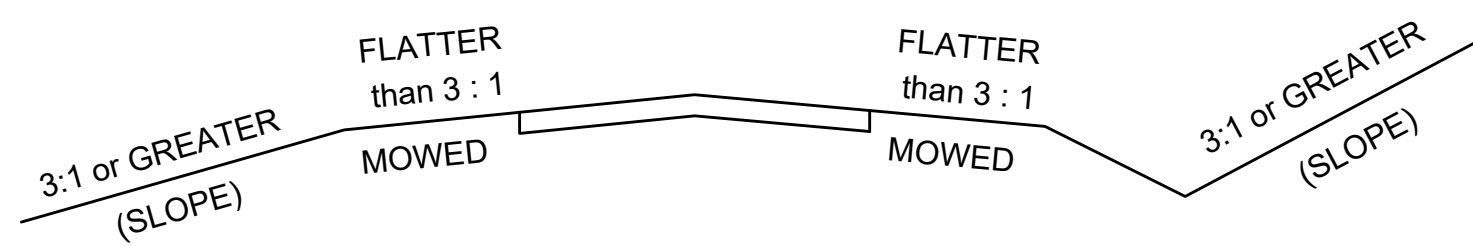
MIX	LBS./ACRES	DESCRIPTION
1	▲ 200	✕ 100% CERTIFIED FINE FESCUE
2	▲	100% CERTIFIED TALL FESCUE
3	▲ 100	50% CERTIFIED TALL FESCUE ✕ 50% CERTIFIED FINE FESCUE
4	▲	50% ORCHARDGRASS 50% CERTIFIED KENTUCKY BLUEGRASS
5	▲	100% BERMUDAGRASS
TEMPORARY		
3/1 - 5/16 and 8/16 - 3/1	50	50% CERTIFIED TALL FESCUE
	50	50% BARLEY, WINTER RYE OR WINTER WHEAT
	50	50% FOXTAIL MILLET
	50	50% CERTIFIED TALL FESCUE

ADDITIVES

TYPE	LBS./ACRES	DESCRIPTION
A	▲	100% LOVEGRASS
B	▲ 20	100% BARLEY, WINTER RYE OR WINTER WHEAT
C	▲ 10	100% FOXTAIL MILLET
D	▲ 10	100% ANNUAL RYEGRASS
E	▲	100% CROWN VETCH (LEGUME)
F	▲	100% SERICEA LESPEDEZA (LEGUME)
G	▲	100% BIRDSFOOT TREFOIL (LEGUME)
H	▲ 10	Little Bluestem
I	▲	
J	▲	
K	▲	

- ▲ ALL RATES TO BE SPECIFIED BY THE DISTRICT ROADSIDE MANAGER
- ✕ FINE FESCUES INCLUDE CHEWINGS, CREEPING RED, HARD, SHEEP

SECTION OF SEED LOCATIONS



SEEDING SCHEDULE

PROJECT NUMBERS	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE	SLOPES SEED MIX WITH ADDITIVE	MOWED SEED MIX WITH ADDITIVE
	SPRING MONTH & DATE	SUMMER MONTH & DATE	FALL & WINTER MONTH & DATE			
URBAN	4/1 - 6/1	6/1 - 9/15	9/15 - 4/1	1D	1D	1B
STANDARD	3D	3D	3D	3CH	3C	3B
✕ SPECIFY KIND OF FINE FESCUE	HARD	HARD	HARD	HARD	HARD	HARD

NOTES:

APPROXIMATELY 0.72 ACRES WILL BE DISTURBED ON THIS PROJECT AND WILL REQUIRE THE ESTABLISHMENT OF GRASSES AND/OR LEGUMES.

☆ NOTES FOR FIELD USE ONLY

OVERSEEDING RATES SHALL BE 100 PERCENT OF THE SEED MIXTURE SUPPLIED WITHOUT FERTILIZER.

THE ENGINEER WILL REQUIRE THE CONTRACTOR TO PERFORM SUPPLEMENTAL SEEDING WHEN LESS THAN 75 PERCENT UNIFORM STAND OF THE PERMANENT GRASS SPECIFIED IN THE MIXTURES IS OBTAINED. (ANNUAL SPECIES SUCH AS, RYE AND MILLET ARE TEMPORARY VARIETIES AND REQUIRE SUPPLEMENTAL SEEDING.)

NOTES APPLY TO SCHEDULE

LEGUME SEED MIXES (BIRDSFOOT TREFOIL, CROWN VETCH, AND SERICEA LESPEDEZA) AND WEEPING LOVEGRASS SHALL NOT BE USED ON SHOULDERS AND OTHER LOCATIONS FLATTER THAN 3:1 SLOPE.

LEGUME SEED SHALL BE INOCULATED WITH THE APPROPRIATE STRAIN AND RATE OF BACTERIA. FOR HYDROSEEDING, USE FIVE TIMES THE DRY SEEDING RATE OF INOCULATE.

A TEMPORARY MIX OR EROSION CONTROL MULCH, AS DIRECTED BY THE ENGINEER, IS TO BE USED ONLY ON AREAS THAT ARE TO BE REGRADED OR LATER DISTURBED, IF LEFT DORMANT FOR MORE THAN 15 DAYS.

EROSION CONTROL MULCH, AS DIRECTED BY THE ENGINEER, IS TO BE USED ON AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN 15 DAYS BETWEEN DECEMBER 1 AND FEBRUARY 28.

EROSION CONTROL MULCH, AS LISTED ON THE VDOT APPROVED PRODUCTS LIST, SHALL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

EROSION CONTROL MULCH SHALL PROVIDE 100 PERCENT COVERAGE OF ALL DENUDED AREAS.

SPRING & SUMMER AND FALL & WINTER DEFINED FOR THE PURPOSE OF DETERMINING WHETHER HULLED OR UNHULLED BERMUDAGRASS AND SERICEA LESPEDEZA SEED IS REQUIRED:

SPRING & SUMMER 4/1 - 9/15 - USE HULLED SEED
FALL & WINTER 9/15 - 4/1 - USE UNHULLED SEED

TYPE I MULCH (STRAW) TO BE USED ON NEWLY SEEDED AREAS ADJACENT TO ALL WATERWAYS, WETLANDS, SWAMPS, OR ANY AREA IN WHICH DRAINAGE FLOWS TOWARD AREAS UNDER THE JURISDICTION OF THE ENVIRONMENTAL REGULATORY AGENCIES.

TYPE I MULCH SHALL BE APPLIED TO PROVIDE A MINIMUM 90 PERCENT COVERAGE.

TYPE I MULCH SHALL BE TACKED WITH FIBER MULCH AT THE RATE OF 750 LBS. PER ACRE AND/OR MULCH TACKIFIER.

TYPE II MULCH (FIBER MULCH) MAY BE SUBSTITUTED FOR TYPE I MULCH AT THE RECOMMENDATION OF THE DISTRICT ROADSIDE MANAGER.

TYPE II MULCH SHALL BE APPLIED AT A RATE OF 1500 LBS. (NET DRY WEIGHT) PER ACRE TO PROVIDE A MINIMUM OF 90 PERCENT COVERAGE, AND SHALL BE APPLIED IN A SEPARATE APPLICATION.

ALL TOPSOIL IS TO BE FREE OF HARD LUMPS, CLODS, ROCKS AND FOREIGN DEBRIS AND IS TO BE HAND RAKED TO TIE INTO EXISTING LAWNS.

ALL SEED MUST BE IN CONFORMANCE WITH VDOT SEED SPECIFICATIONS FOR GRASSES & LEGUMES AND BE PROVIDED AT THE PROJECT SITE IN BAGS NOT OPENED AND LABELED FOR USE ON VDOT PROJECTS WITH A GREEN TAG CERTIFYING INSPECTION BY THE VIRGINIA CROP IMPROVEMENT ASSOCIATION.

MIX REQUIREMENTS THIS PROJECT

- MIX 1 (URBAN) SHALL BE USED FOR COMMERCIAL/RESIDENTIAL/CHURCH PROPERTIES TO MORE CLOSELY MATCH THOSE TYPICALLY MORE MANICURED LAWNS. MIX 3 (STANDARD) IS TO BE USED WHERE MIX 1 IS NOT USED.

- THREE (3) INCHES OF TOPSOIL SHALL BE USED WHENEVER MIX 1 IS USED, AND FOR ANY SHOULDER/DITCH/MEDIAN/STORMWATER MANAGEMENT AREA (MIX 3). EC-2 EROSION CONTROL MATTING SHALL BE USED WHEN MIX 1 IS USED IN FRONT OF COMMERCIAL/RESIDENTIAL/CHURCH PROPERTIES, WHERE THE SLOPE WILL BE STEEPER THAN 3:1.

ROADSIDE DEVELOPMENT SUMMARY

PROJECT NUMBERS	TOPSOIL 2" CLASS A B	REGULAR SEED	OVER SEEDING	LIME	FERT. 15-30-15	LEGUME SEED	LEGUME OVER SEEDING	TEMPORARY SEEDING
	ACRES	LBS.	LBS.	TONS	TONS	LBS.	LBS.	LBS.

⊗ DENOTES ITEM(S) TO BE PAID FOR ON BASIS OF PLAN QUANTITIES IN ACCORDANCE WITH CURRENT ROAD AND BRIDGE SPECIFICATIONS.

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REVISION DESCRIPTION	DATE
	04/28/2020

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DESIGNED BY: IJH
CHECKED BY: NJS
SCALE: 1"=25'

TIMMONS GROUP

FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
ROADSIDE DEVELOPMENT SHEET

JOB NO. 41013.009
SHEET NO. 02E

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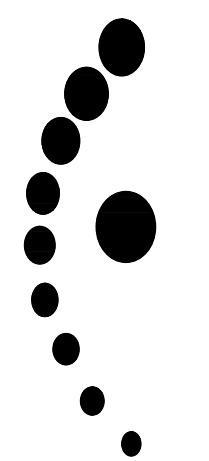
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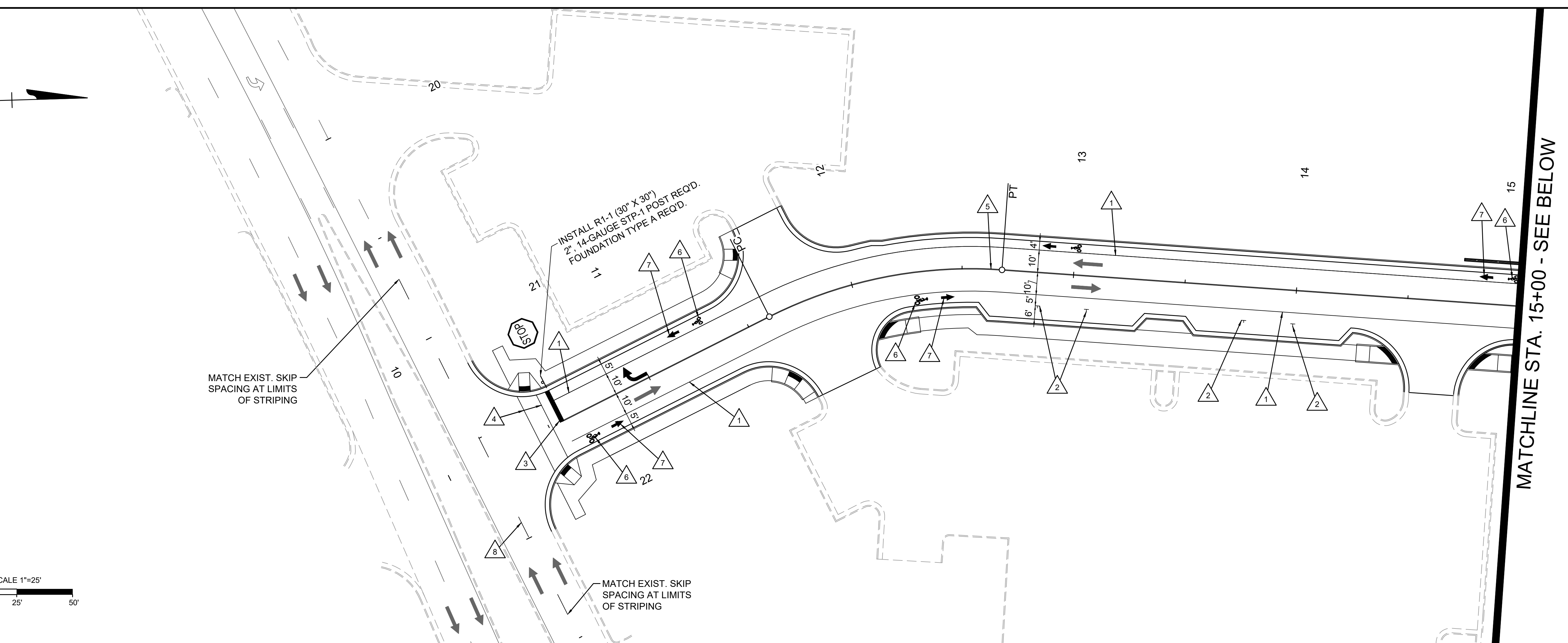
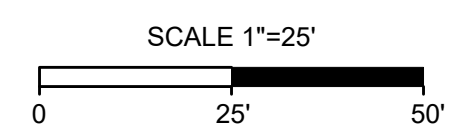
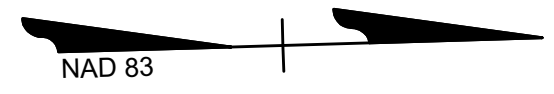
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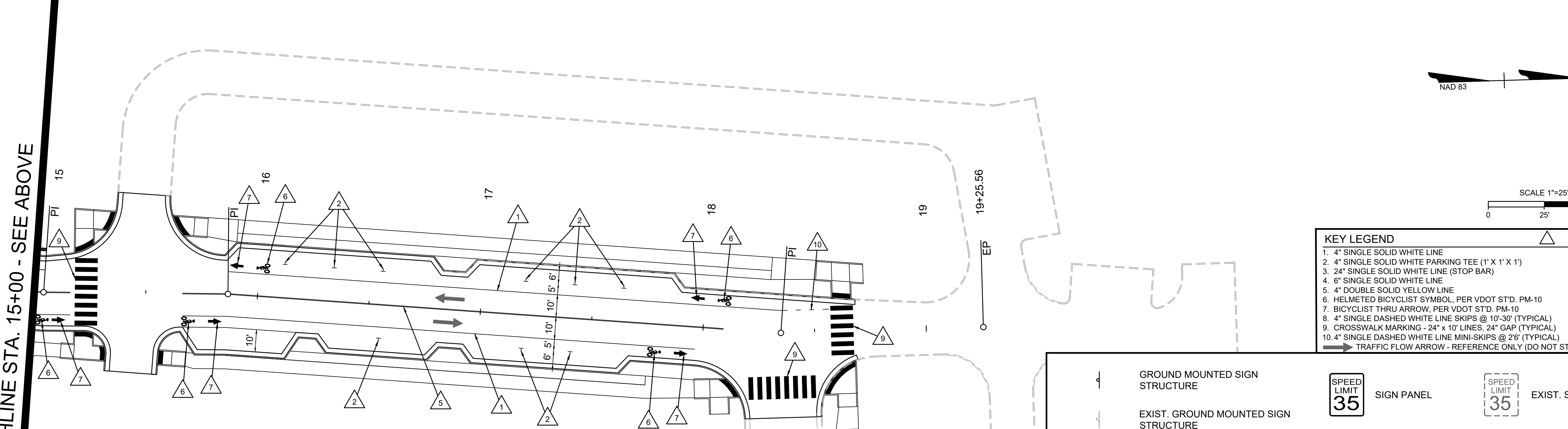
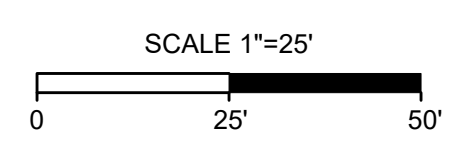
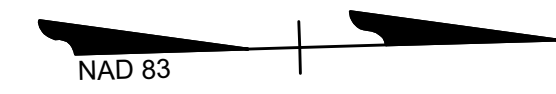
TIMMONS GROUP
FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
PAVEMENT MARKING & SIGNING PLAN

JOB NO. 41013.009
SHEET NO. 02G

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MATCHLINE STA. 15+00 - SEE BELOW



PAVEMENT MARKING AND SIGNING NOTES

- ALL SIGNING AND PAVEMENT MARKING WORK SHALL BE IN CONFORMANCE WITH THE FOLLOWING DOCUMENTS:
 - 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), REVISION 1 AND 2
 - 2011 VIRGINIA SUPPLEMENT TO THE 2009 MUTCD, REVISION 1
 - 2016 VDOT ROAD AND BRIDGE STANDARDS
 - 2016 VDOT ROAD AND BRIDGE SPECIFICATIONS
- UNLESS OTHERWISE APPROVED BY THE ENGINEER OR INDICATED IN THE MAINTENANCE OF TRAFFIC AND SEQUENCE OF CONSTRUCTION PLANS, EXISTING TRAFFIC SIGNS WHICH ARE TO BE REMOVED/RELOCATED SHALL REMAIN IN PLACE UNTIL THE NEW SIGN STRUCTURE IS IN PLACE.
- THE REMOVAL, RELOCATION, OR MODIFICATION OF EXISTING SIGN PANELS, STRUCTURES, OR FOUNDATIONS SHALL CONFORM TO SECTION 510 OF THE SPECIFICATIONS.
- ALL EXISTING AND PROPOSED SIGN LOCATIONS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR. ALL PROPOSED SIGN LOCATIONS SHALL BE STAKED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
- PAVEMENT MARKINGS SHALL BE SPOTTED (USING CHALK OR APPROVED EQUAL) AND FIELD VERIFIED BY VDOT OR THE ENGINEER PRIOR TO APPLYING THE THERMOPLASTIC MARKINGS.
- VDOT TRAFFIC ENGINEERING SECTION WILL NEED TO BE CONTACTED 72 HOURS PRIOR TO ANY PAVEMENT MARKING ACTIVITIES. FAILURE TO DO SO CAN RESULT IN ADDITIONAL COSTS TO THE CONTRACTOR.
- ALL PERMANENT MARKINGS SHALL BE TYPE B, CLASS 1 UNLESS OTHERWISE DIRECTED BY VDOT TRAFFIC ENGINEERING.
- ALL PAVEMENT MARKINGS MUST HAVE A PERSON WITH A PAVEMENT MARKING CERTIFICATION ISSUED BY THE VDOT MATERIALS DIVISION ON SITE DURING PAVEMENT MARKING OPERATIONS.
- FOR ALL PROPOSED AND RELOCATED SIGNS, THE BOTTOM OF SIGN HEIGHT MUST BE AT LEAST 7' ABOVE GROUND (EXCEPT OM-3 SIGNS).
- ALL REPLACED OR RELOCATED SIGNS SHALL BE INSTALLED AS NEW SIGNS. EXISTING SIGNS SHALL NOT BE RE-USED.
- CONTRACTOR SHALL TIE ALL PROPOSED PAVEMENT MARKINGS TO EXISTING MARKINGS AT THE PROJECT LIMITS
- CONTRACTOR SHALL BE RESPONSIBLE FOR ERADICATION OF ALL EXISTING PAVEMENT MARKINGS WHICH ARE IN CONFLICT WITH PROPOSED STRIPING PLAN.
- ALL PAVEMENT MARKINGS WITHIN VDOT RIGHT-OF-WAY SHALL ADHERE TO VDOT PM-3 STANDARDS.

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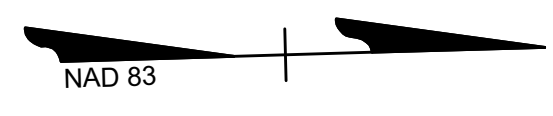
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DESIGNED BY	IJH			
CHECKED BY	NJS			
SCALE	1"=25'			

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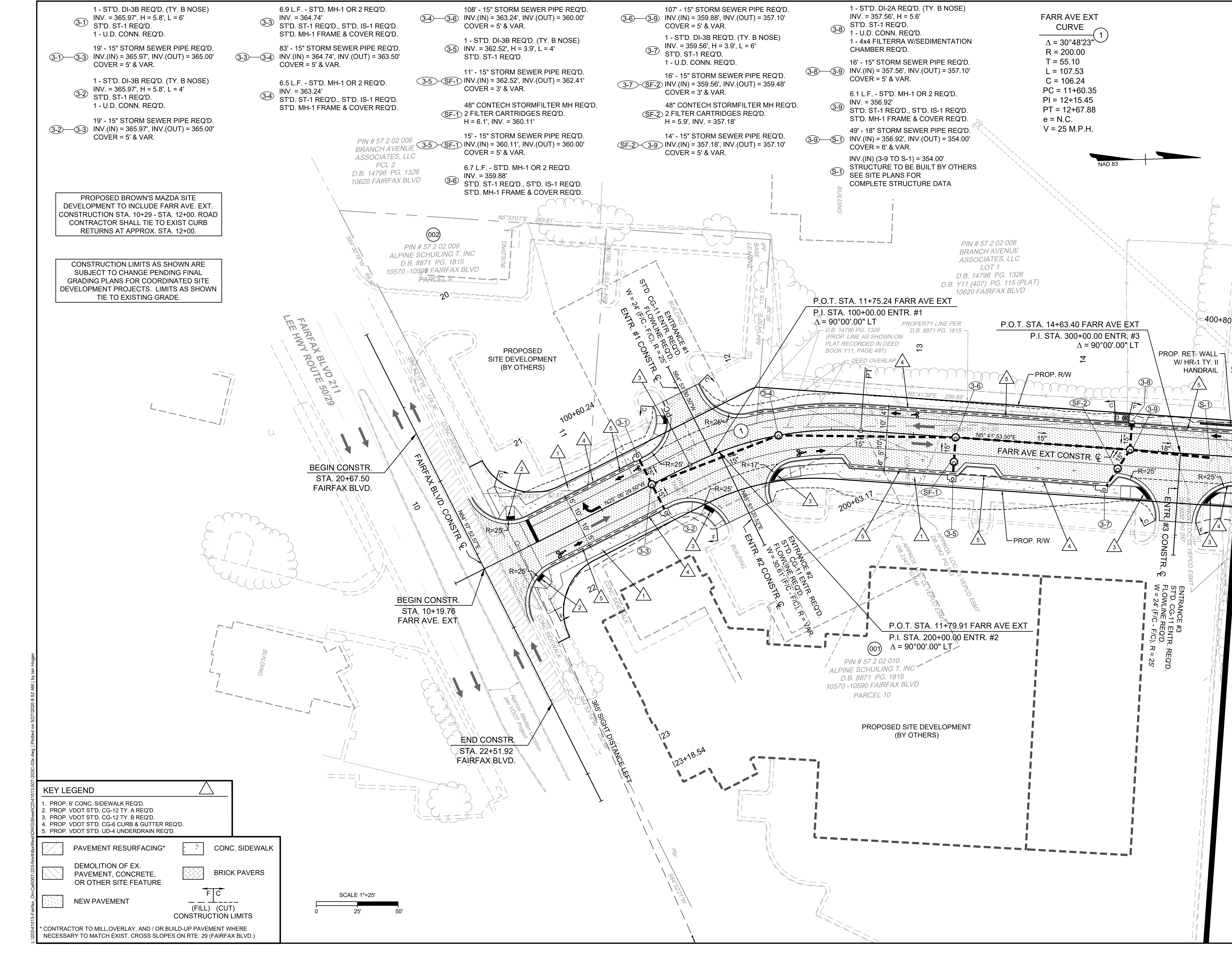
FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
PLAN SHEET

JOB NO. 41013.009
SHEET NO. 03

FARR AVE EXT CURVE
 $\Delta = 30^\circ 48' 23"$
 $R = 200.00$
 $T = 55.10$
 $L = 107.53$
 $C = 106.24$
 $PC = 11+60.35$
 $PI = 12+15.45$
 $PT = 12+67.88$
 $e = N.C.$
 $V = 25 \text{ M.P.H.}$



MATCHLINE STA. 15+00 - SEE SHEET 04



PROPOSED BROWN'S MAZDA SITE DEVELOPMENT TO INCLUDE FARR AVE. EXT. CONSTRUCTION STA. 10+29 - STA. 12+00. ROAD CONTRACTOR SHALL TIE TO EXIST CURB RETURNS AT APPROX. STA. 12+00.

CONSTRUCTION LIMITS AS SHOWN ARE SUBJECT TO CHANGE PENDING FINAL GRADING PLANS FOR COORDINATED SITE DEVELOPMENT PROJECTS. LIMITS AS SHOWN TIE TO EXISTING GRADE.

KEY LEGEND

	PAVEMENT RESURFACING*		CONC. SIDEWALK
	DEMOLITION OF EX. PAVEMENT, CONCRETE, OR OTHER SITE FEATURE		BRICK PAVERS
	NEW PAVEMENT		CONSTRUCTION LIMITS

* CONTRACTOR TO MILL, OVERLAY, AND / OR BUILD-UP PAVEMENT WHERE NECESSARY TO MATCH EXIST. CROSS SLOPES ON RTE. 29 (FAIRFAX BLVD.)

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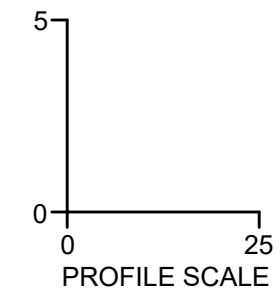
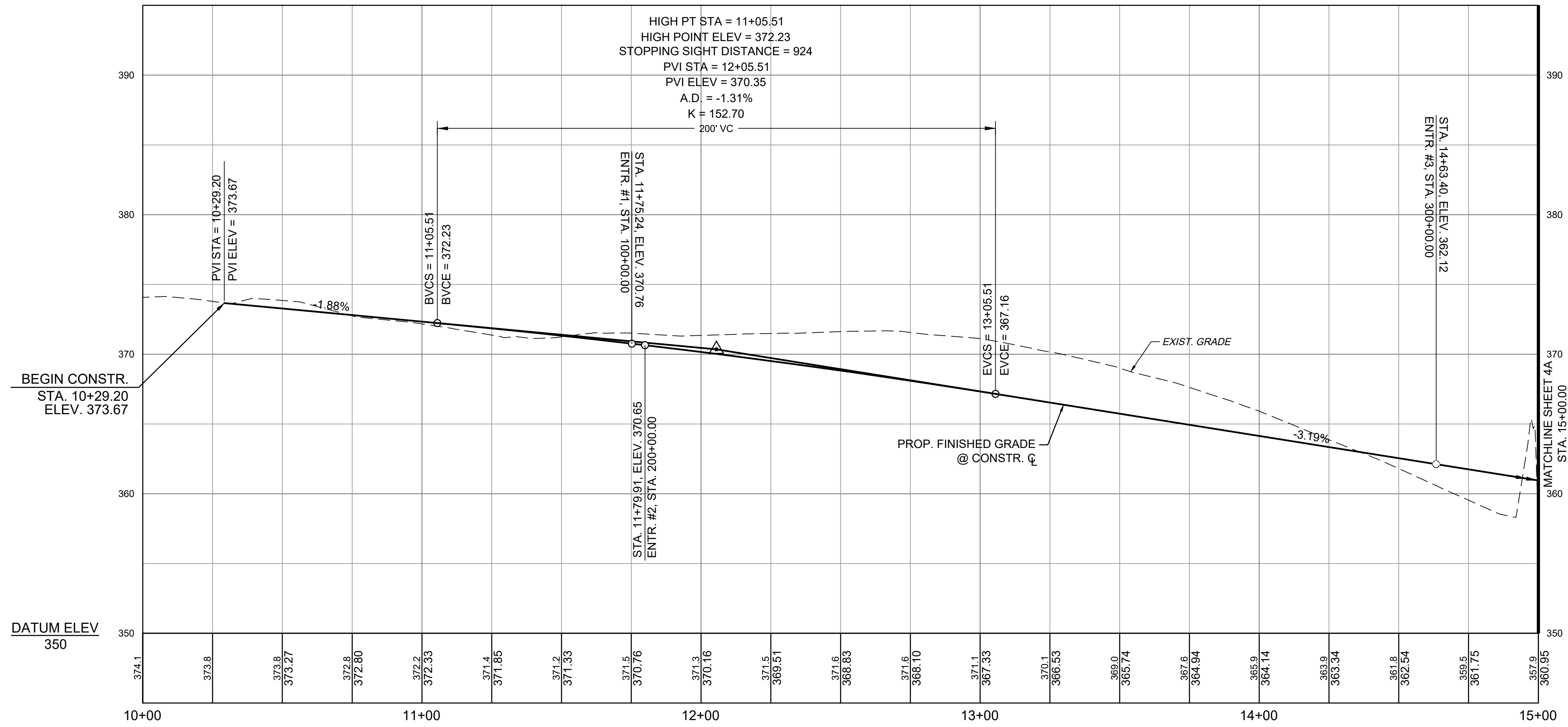
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FARR AVENUE EXT. (NORTHFAX WEST)
 CITY OF FAIRFAX - VIRGINIA

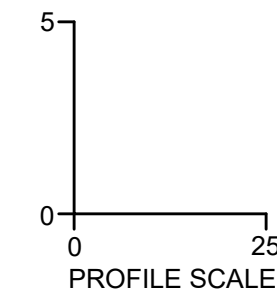
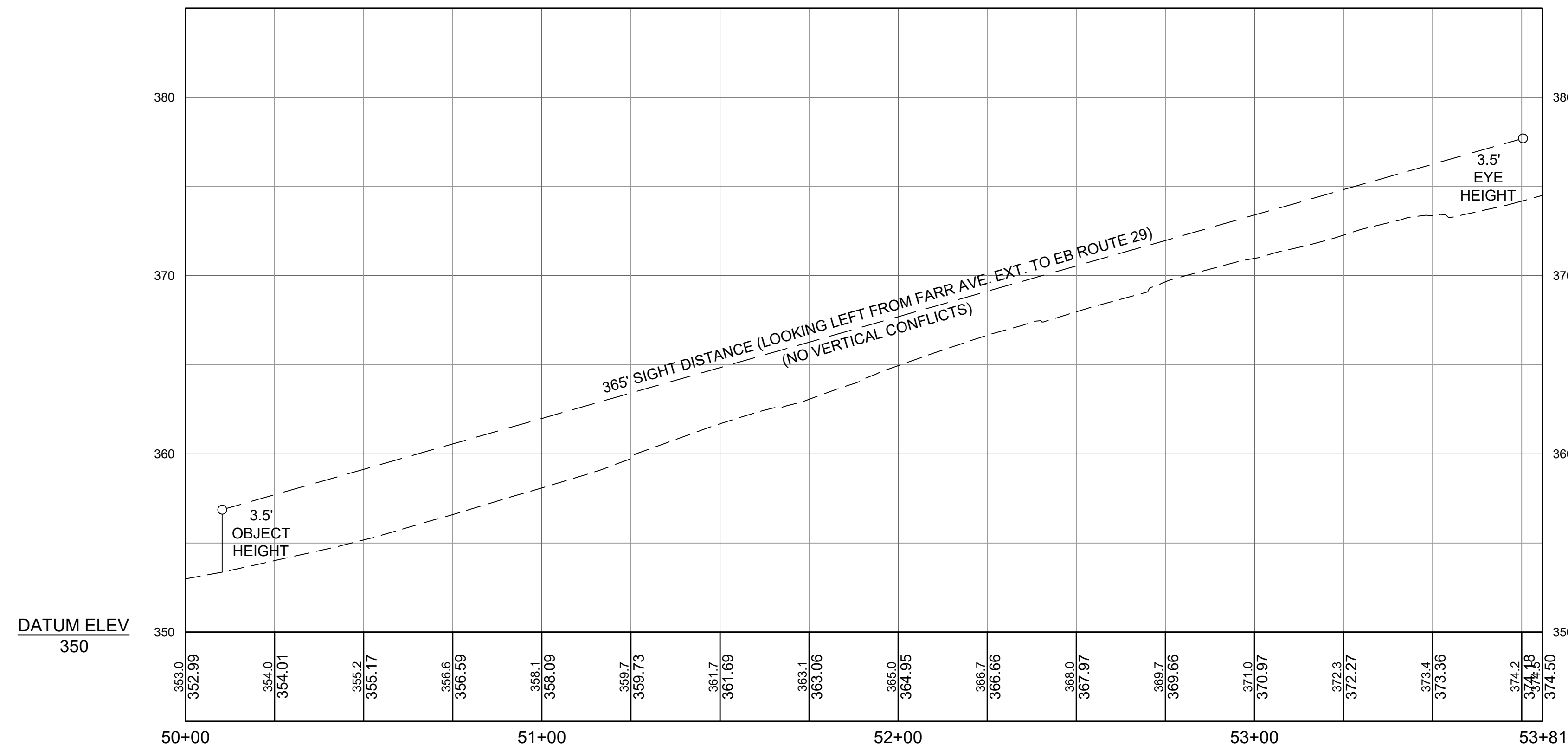
PROFILE SHEET

JOB NO. 41013.009
 SHEET NO. 03A

PROPOSED FINISHED GRADE PROFILE - FARR AVENUE EXTENSION



PROPOSED SIGHT DISTANCE PROFILE - FARR AVENUE EXTENSION

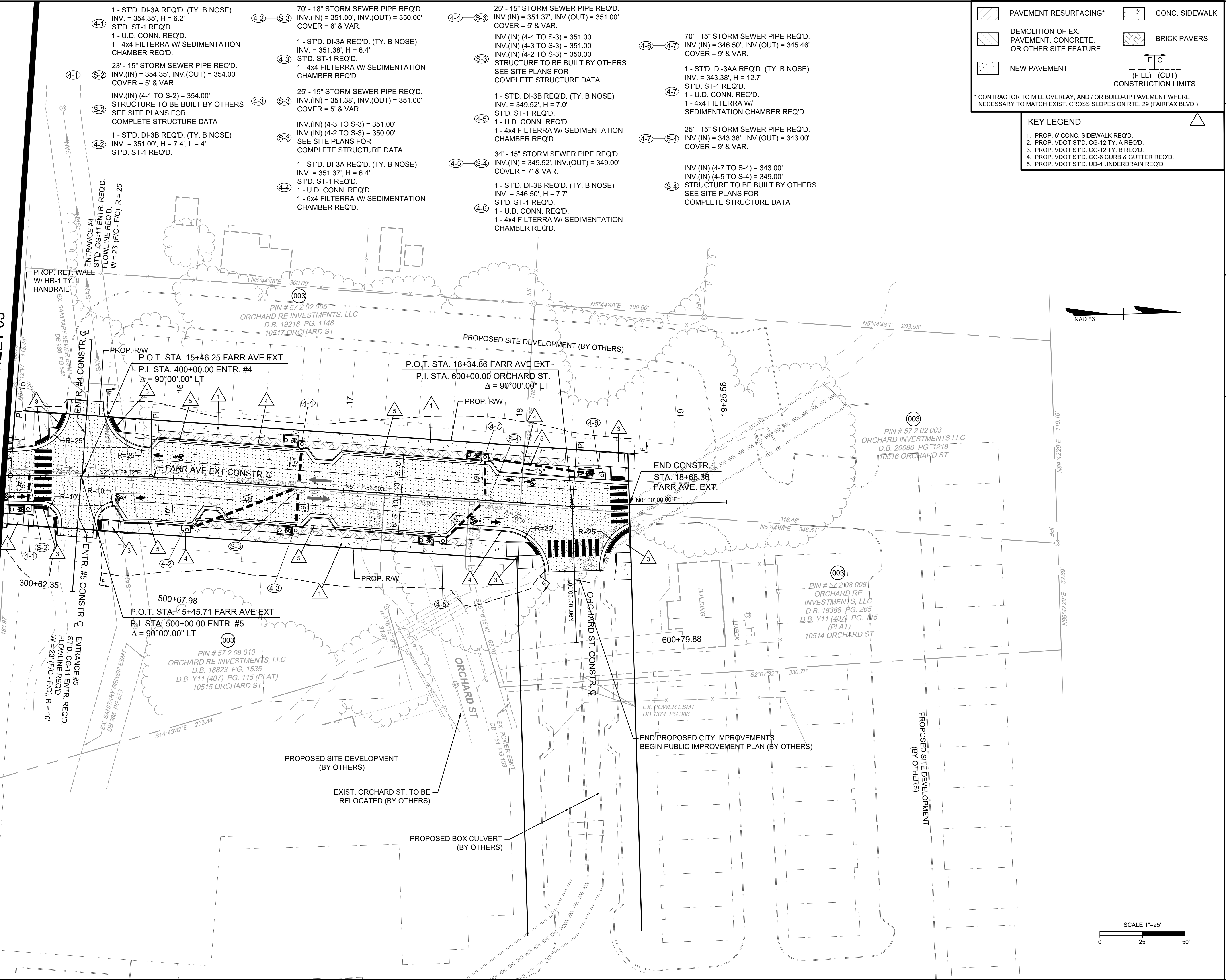


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MATCHLINE STA. 15+00 - SEE SHEET 03



	PAVEMENT RESURFACING*		CONC. SIDEWALK
	DEMOLITION OF EX. PAVEMENT, CONCRETE, OR OTHER SITE FEATURE		BRICK PAVERS
	NEW PAVEMENT	 (FILL) (CUT) CONSTRUCTION LIMITS	

* CONTRACTOR TO MILL/OVERLAY, AND / OR BUILD-UP PAVEMENT WHERE NECESSARY TO MATCH EXIST. CROSS SLOPES ON RTE. 29 (FAIRFAX BLVD.)

- KEY LEGEND**
- PROP. 6" CONC. SIDEWALK REQ'D.
 - PROP. VDOT ST'D. CG-12 TY. A REQ'D.
 - PROP. VDOT ST'D. CG-12 TY. B REQ'D.
 - PROP. VDOT ST'D. CG-6 CURB & GUTTER REQ'D.
 - PROP. VDOT ST'D. UD-4 UNDERDRAIN REQ'D.

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SCALE	1"=25'

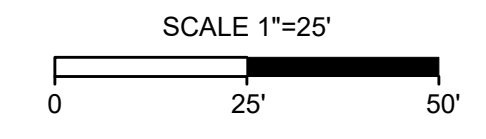
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FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA

PLAN SHEET

JOB NO.	41013.009
SHEET NO.	04

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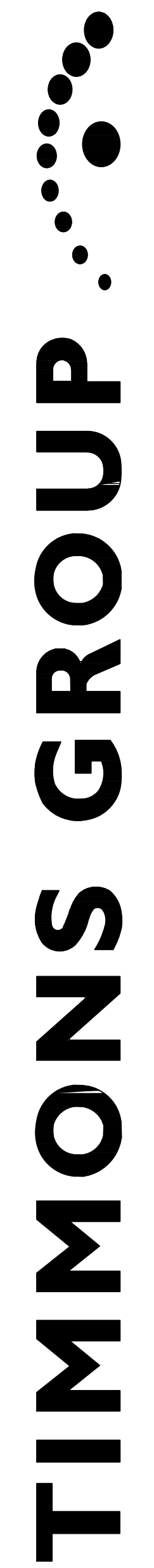
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Site Development | Residential | Infrastructure | Technology

DESIGNED BY
IJH

CHECKED BY
NJS

SCALE
1"=25'

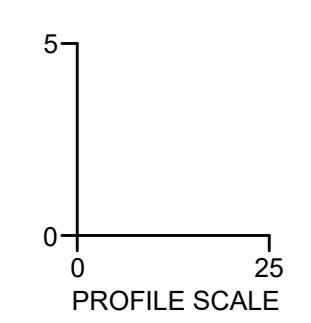
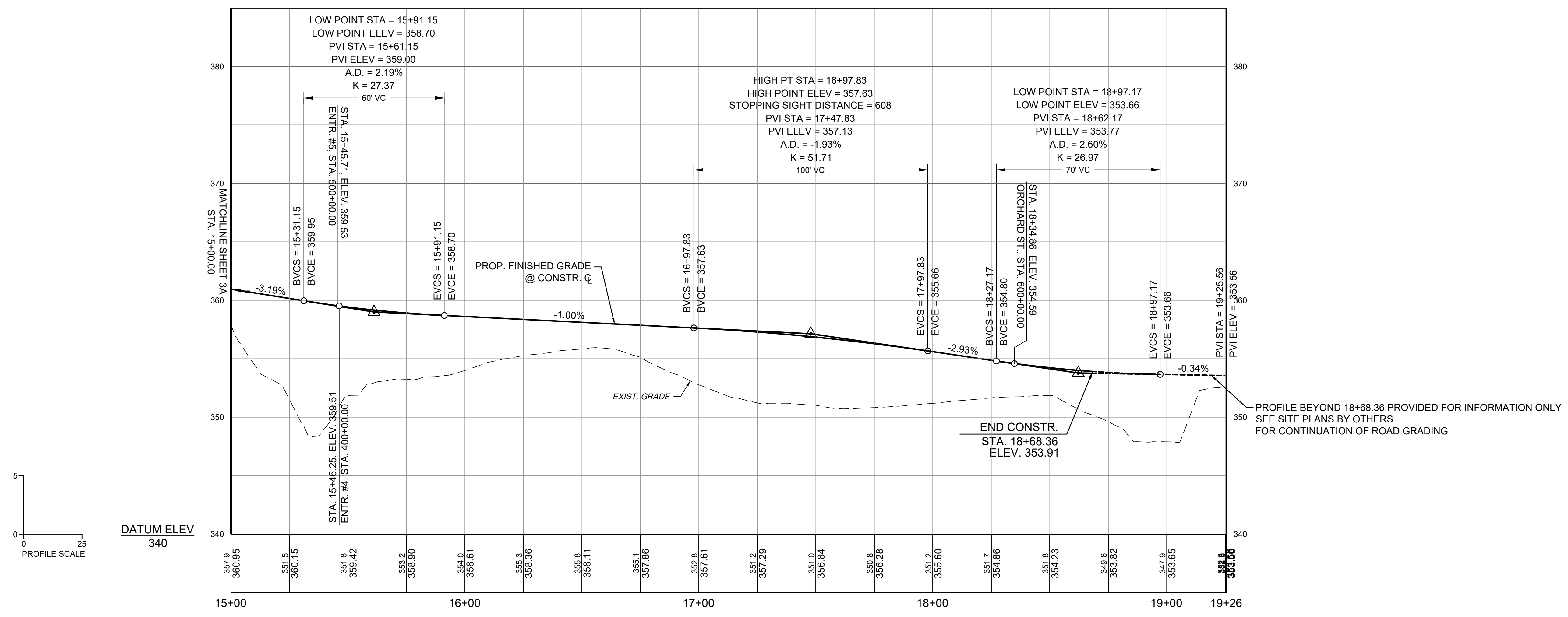


FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
PROFILE SHEET

JOB NO.
41013.009

SHEET NO.
04A

PROPOSED FINISHED GRADE PROFILE - FARR AVENUE EXTENSION

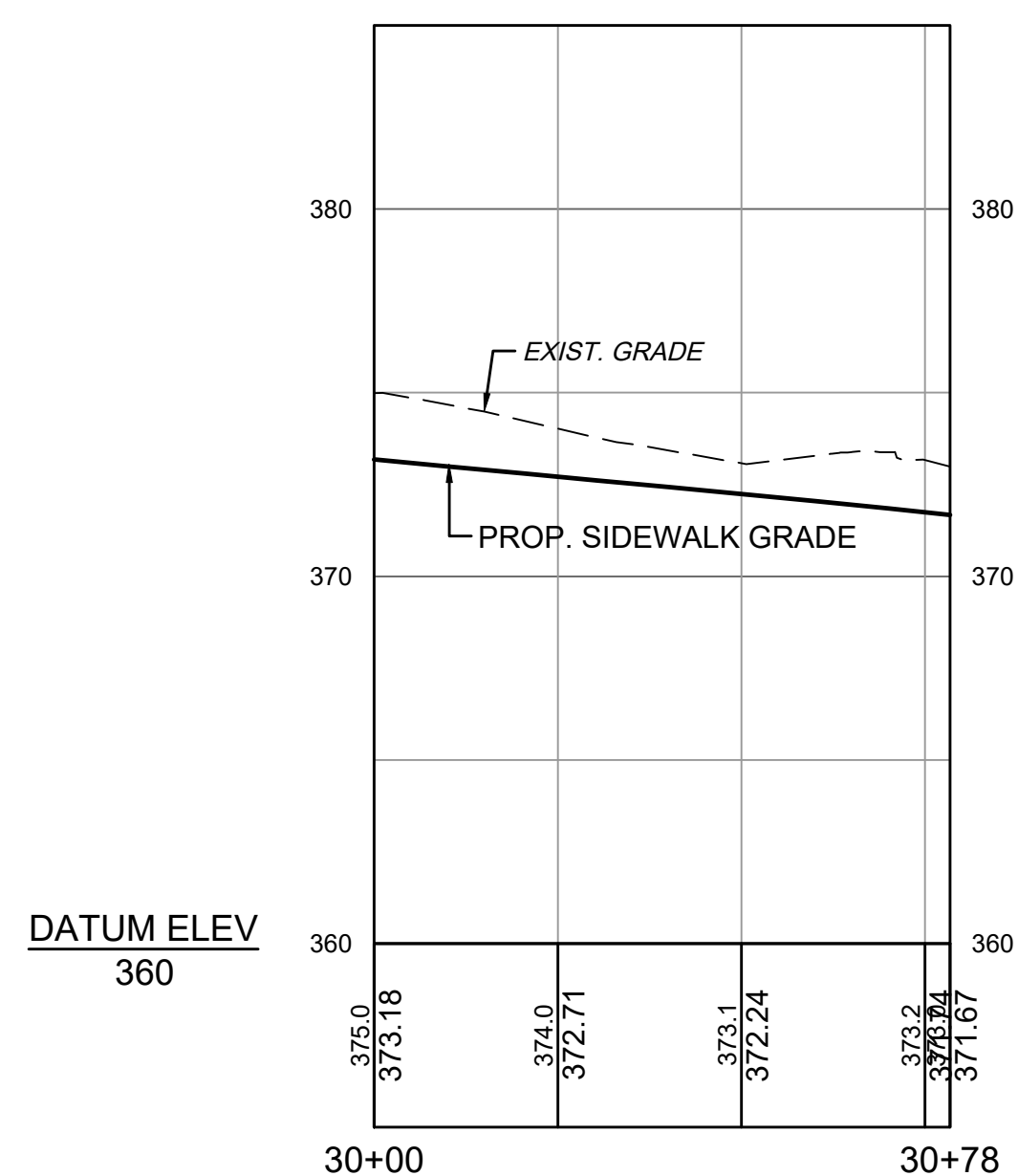


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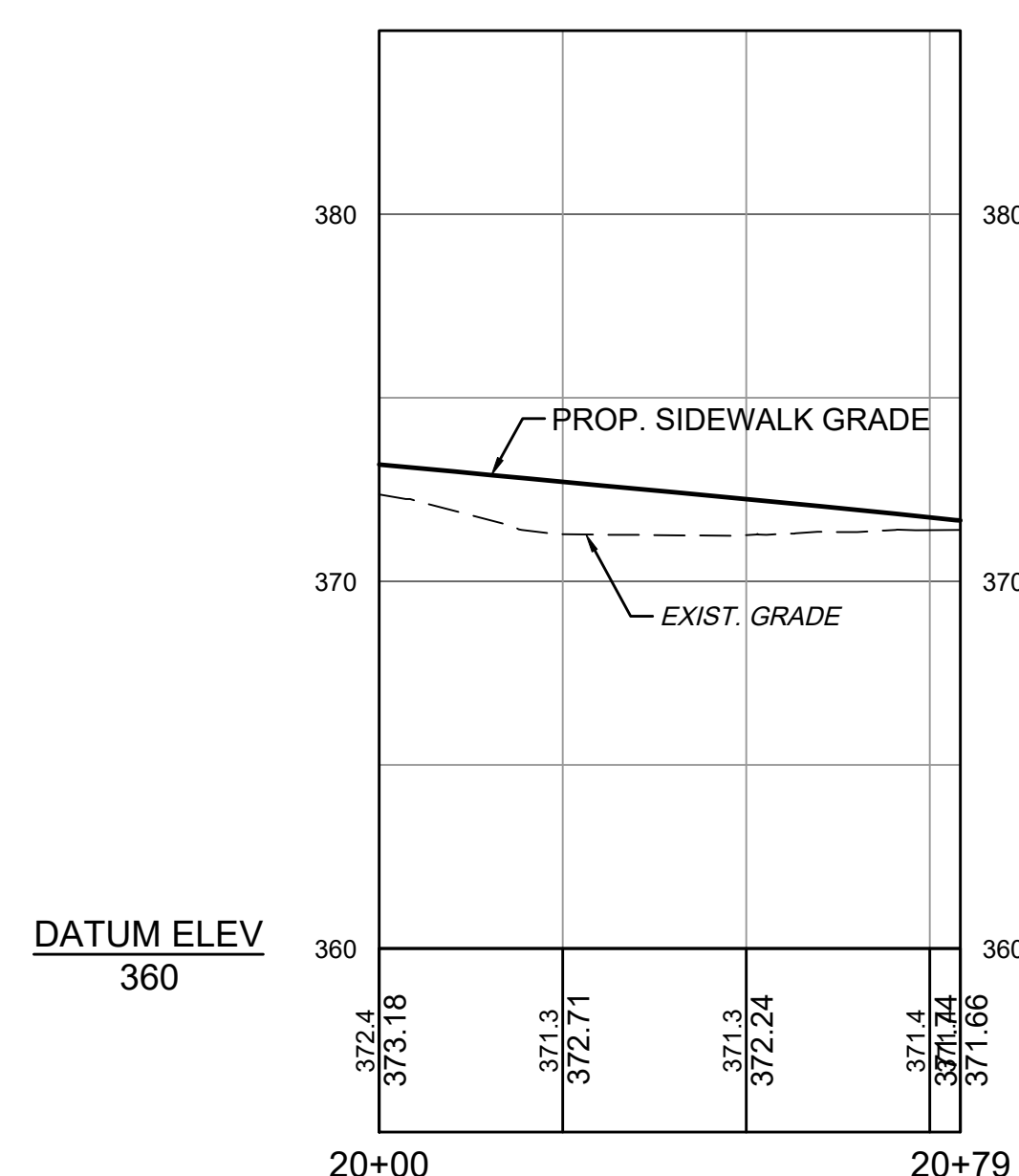
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SIDEWALK PROFILE - RTE. 29 TO ENTR. #1 (LEFT)

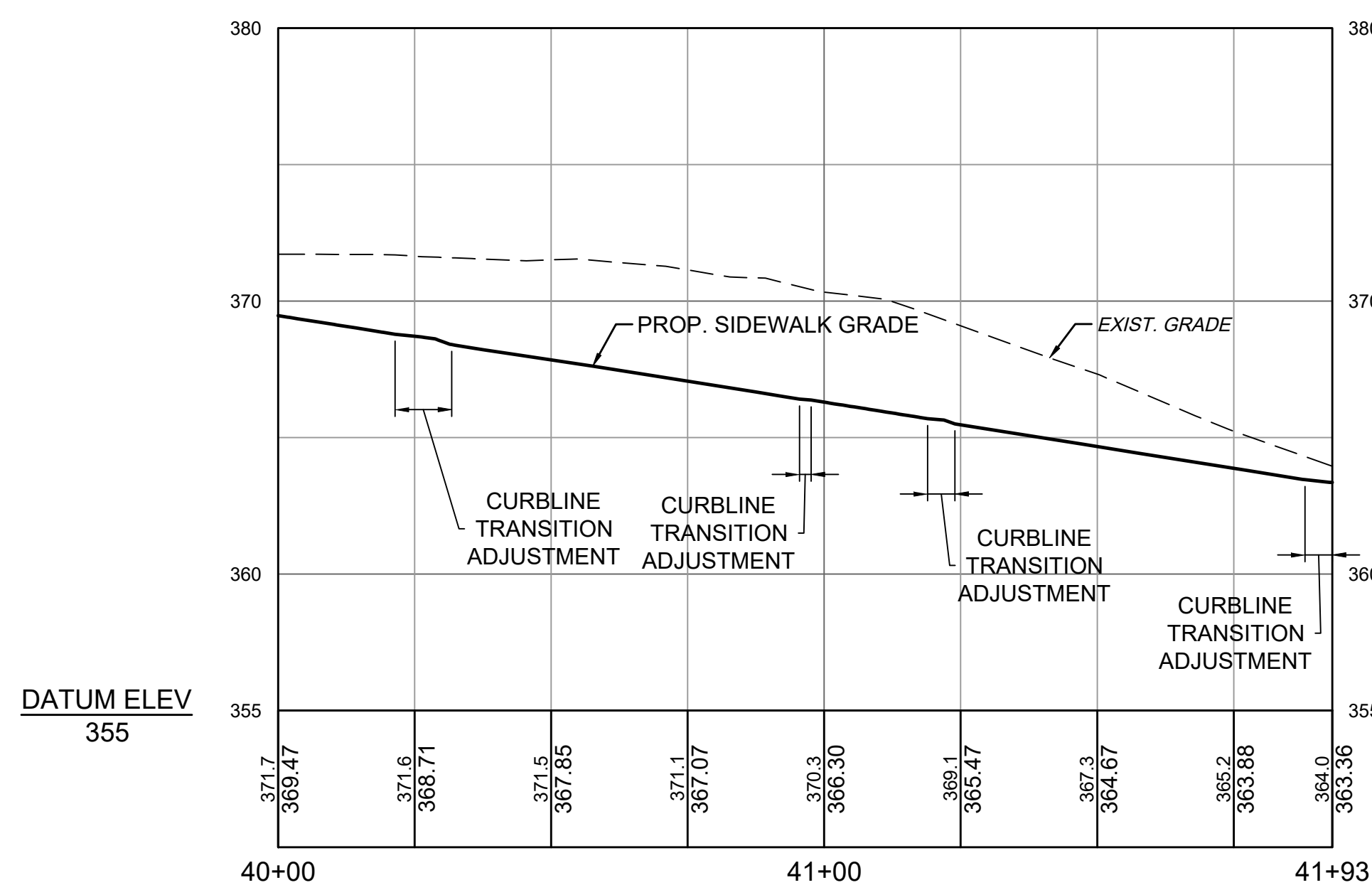


SIDEWALK PROFILE - RTE. 29 TO ENTR. #2 (RIGHT)

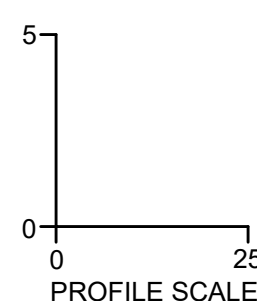
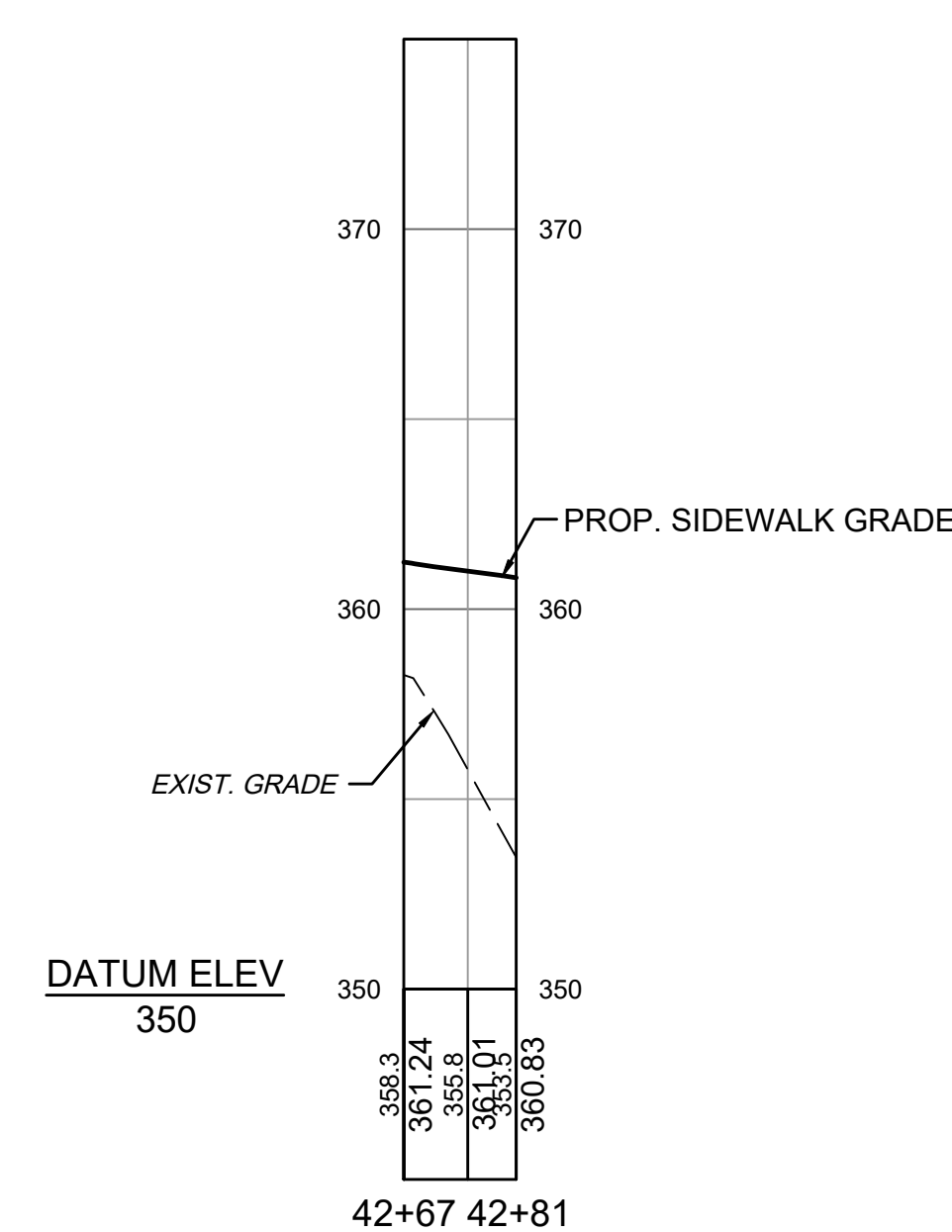


- NOTES:
- SIDEWALK PROFILES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY, TO PROVIDE CONTEXT TO SIDEWALK GRADING AND HIGHLIGHT AREAS WHERE SIDEWALK GRADE DEVIATES FROM FINISHED GRADE CENTERLINE PROFILE. SEE CROSS SECTIONS FOR ELEVATIONS OF FRONT AND BACK OF SIDEWALK.
 - CONTRACTOR SHALL TAKE NOTE THAT SIDEWALK PROFILES MAY VARY FROM CENTERLINE FINISHED GRADE PROFILE THROUGHOUT CURBLINE TRANSITIONS OF BUMP-OUTS. CONTRACTOR SHALL ADJUST GRADE OF SIDEWALK AS REQUIRED TO MEET GRADES SHOWN ON CROSS SECTIONS AND ACHIEVE TYPICAL SECTION. LONGITUDINAL SLOPE OF SIDEWALK SHALL REMAIN BETWEEN 0.40% AND 8% THROUGH THESE TRANSITIONS.
 - ALL SIDEWALK PROFILES SHOWN ON THIS PLAN ARE BASED ON THE BACK (OUTSIDE) OF SIDEWALK.
 - PROFILES SHOWN DO NOT INCLUDE CURB RAMPS OR AREAS WITHIN RADIAL RETURNS OF ENTRANCES OR INTERSECTIONS.

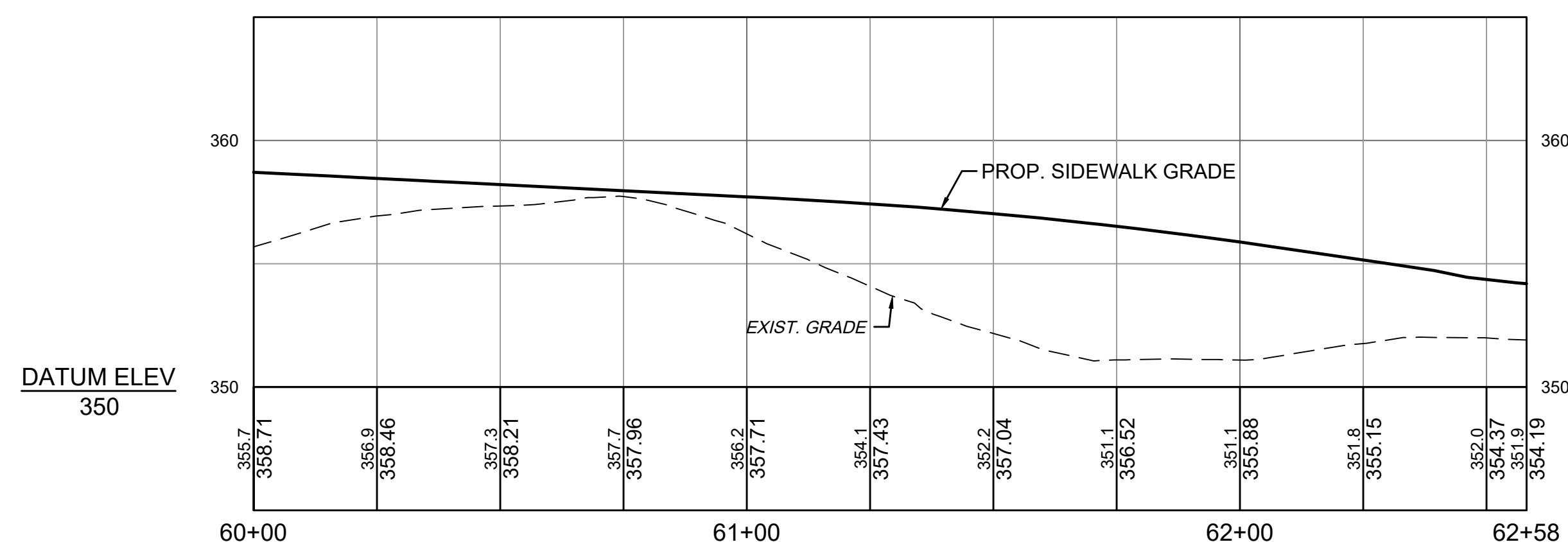
SIDEWALK PROFILE - ENTR. #2 TO ENTR. #3 (RIGHT)



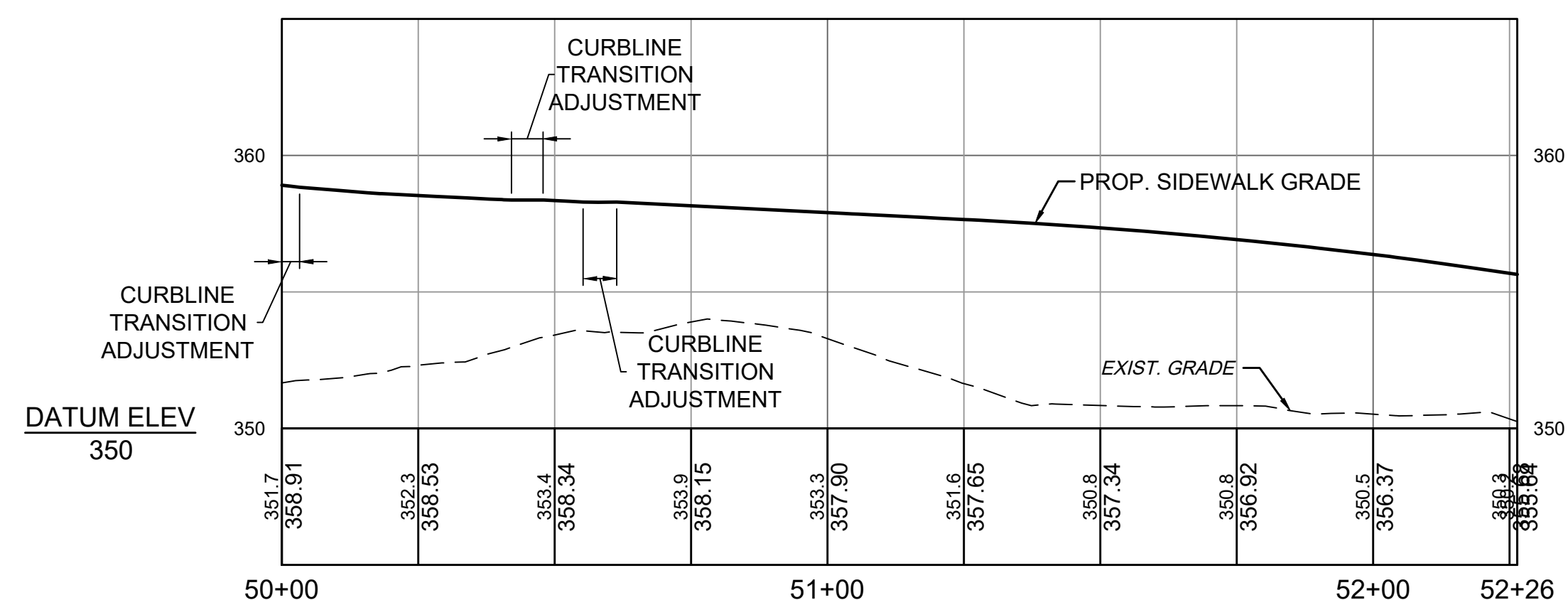
SIDEWALK PROFILE ENTR. #3 TO ENTR. #5 (RIGHT)



SIDEWALK PROFILE - ENTR. #3 TO ORCHARD ST. (LEFT)



SIDEWALK PROFILE - ENTR. #4 TO ORCHARD ST. (RIGHT)



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			CHECKED BY	NJS
			SCALE	1"=25'

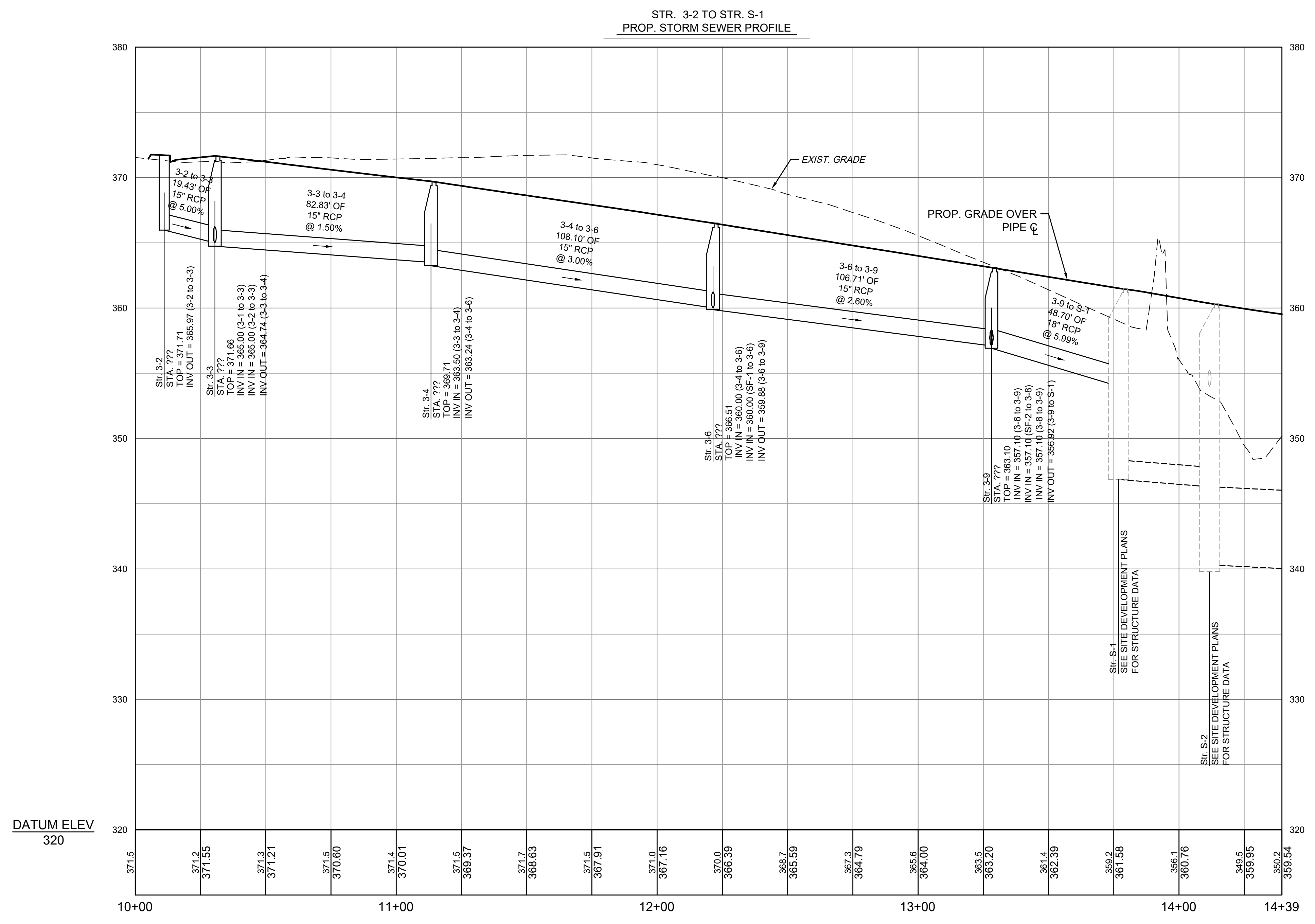
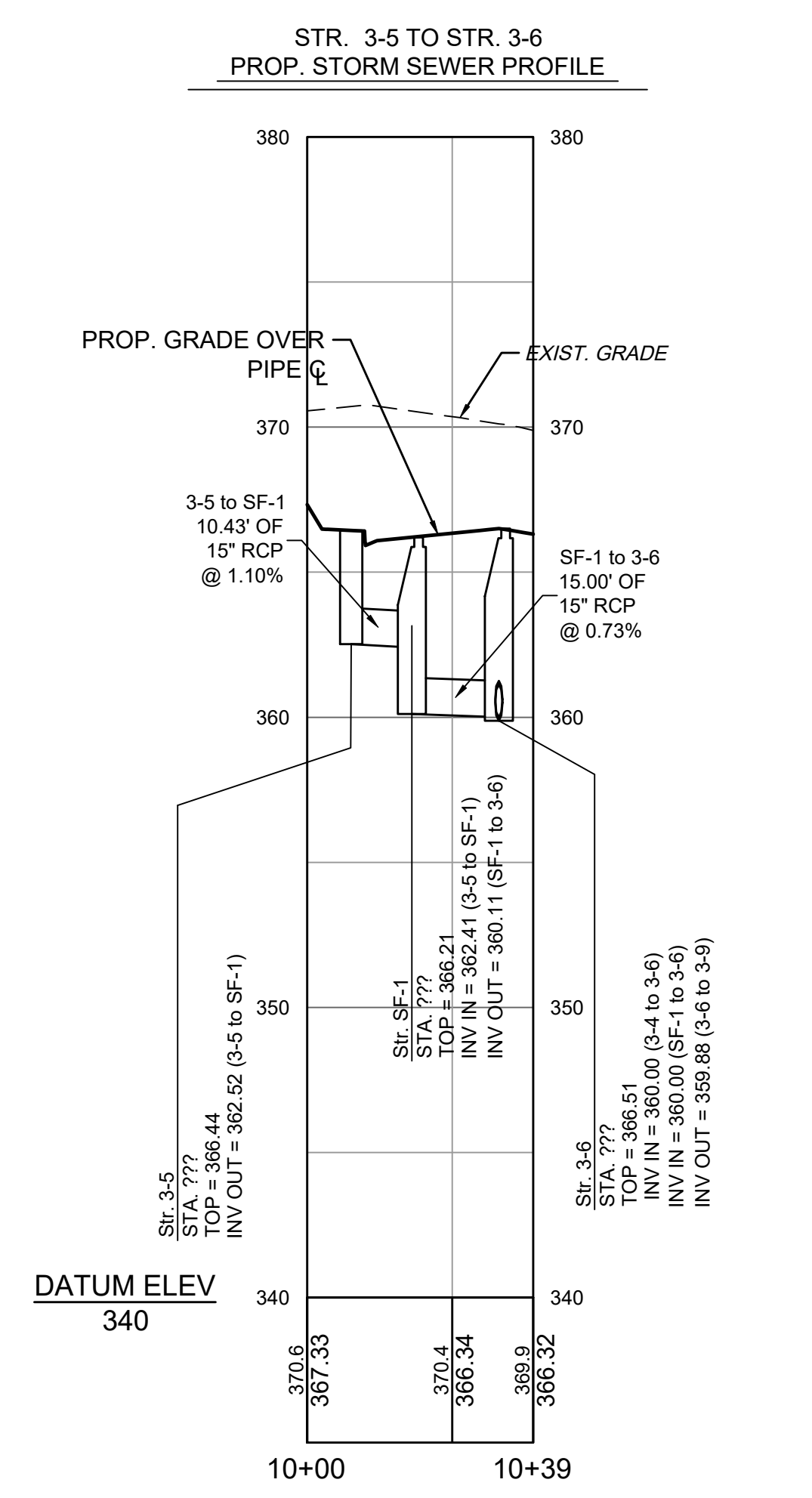
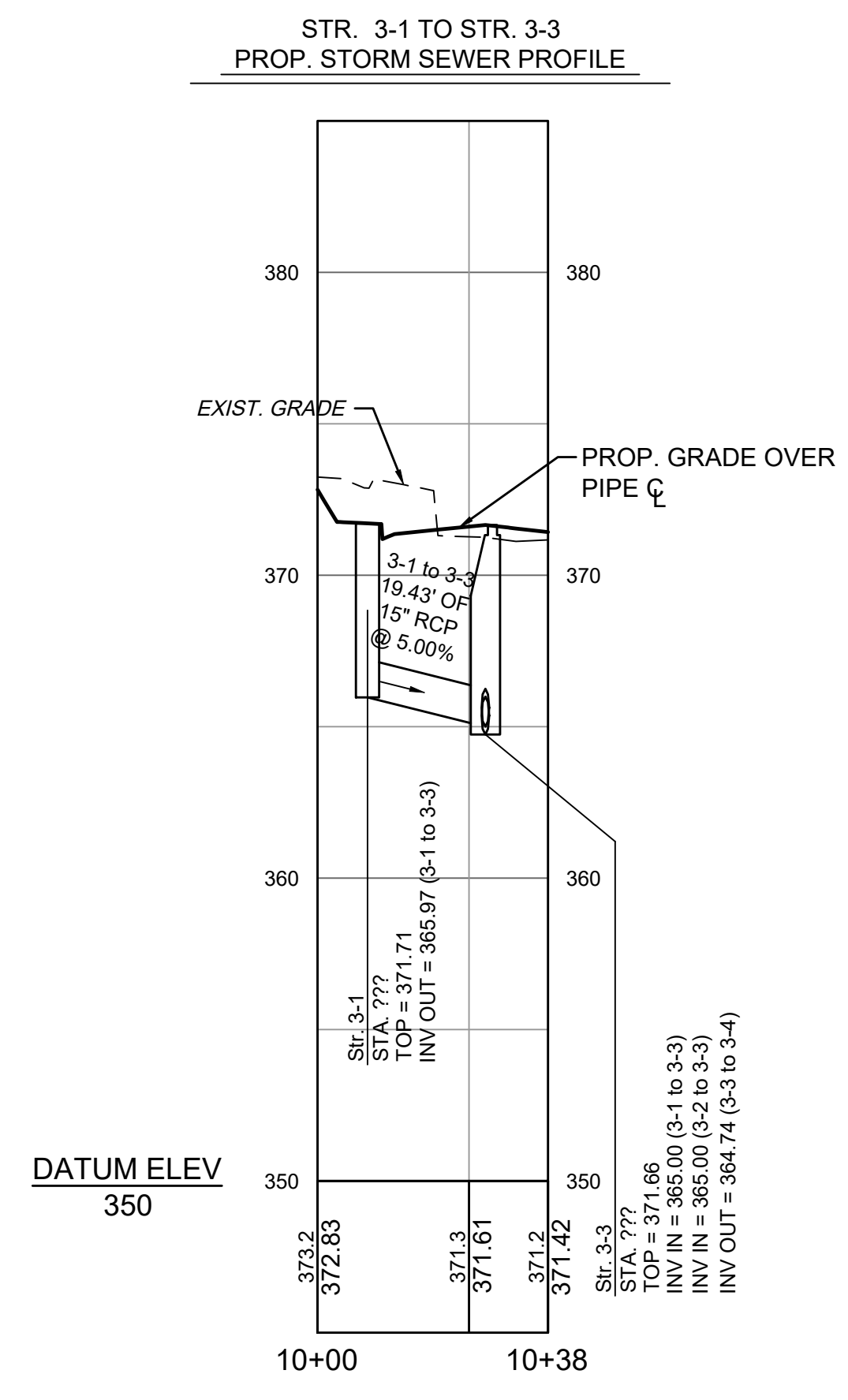
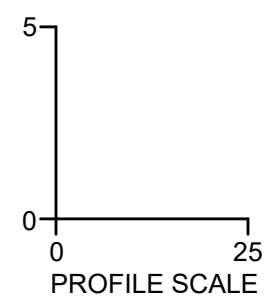
FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA
SIDEWALK PROFILES

JOB NO. 41013.009
SHEET NO. 05

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CHECKED BY: *NJS*
SCALE: 1"=25'

FARR AVENUE EXT. (NORTHFAX WEST)
CITY OF FAIRFAX - VIRGINIA

STORM SEWER PROFILES

JOB NO. 41013.009
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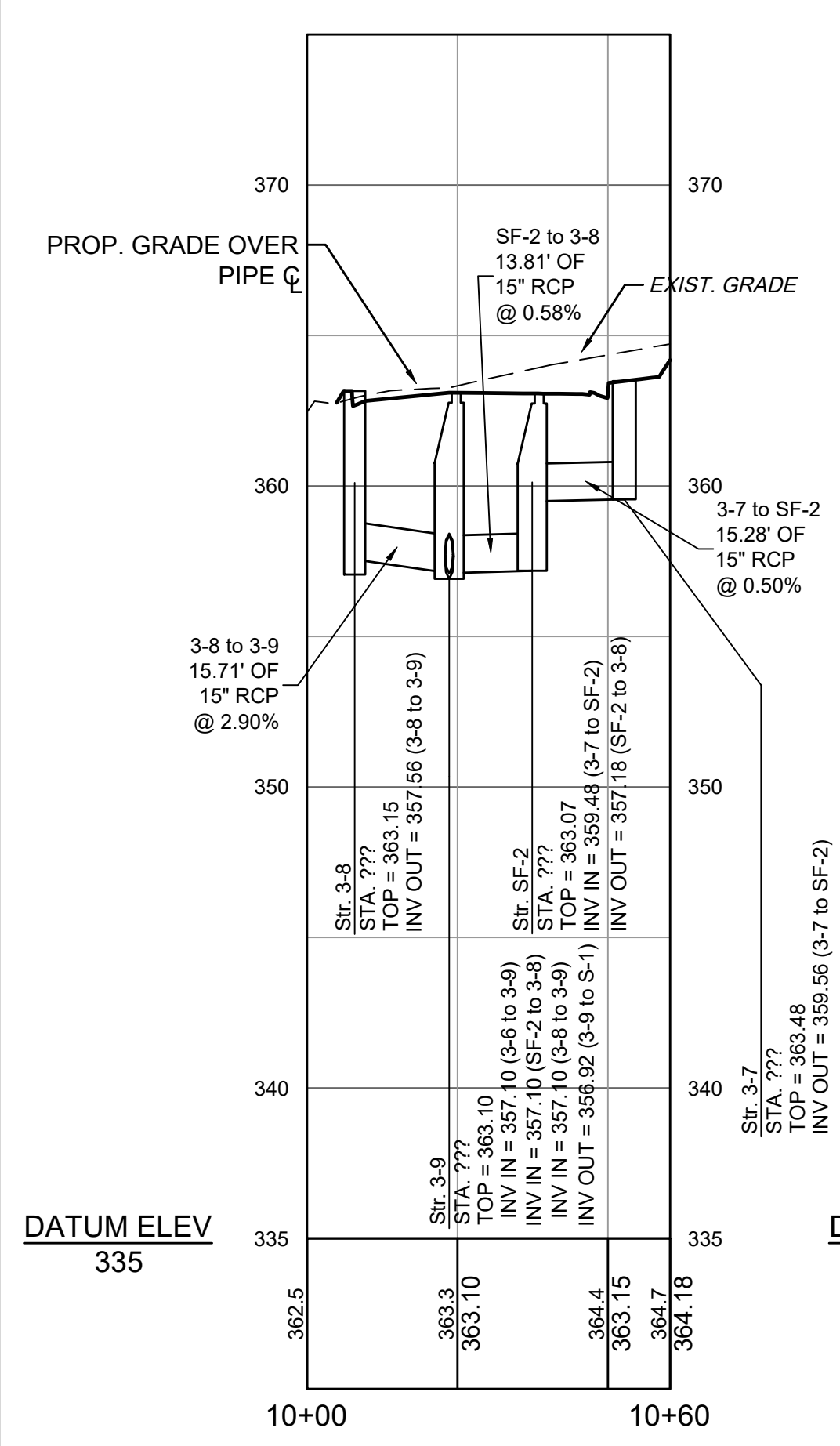
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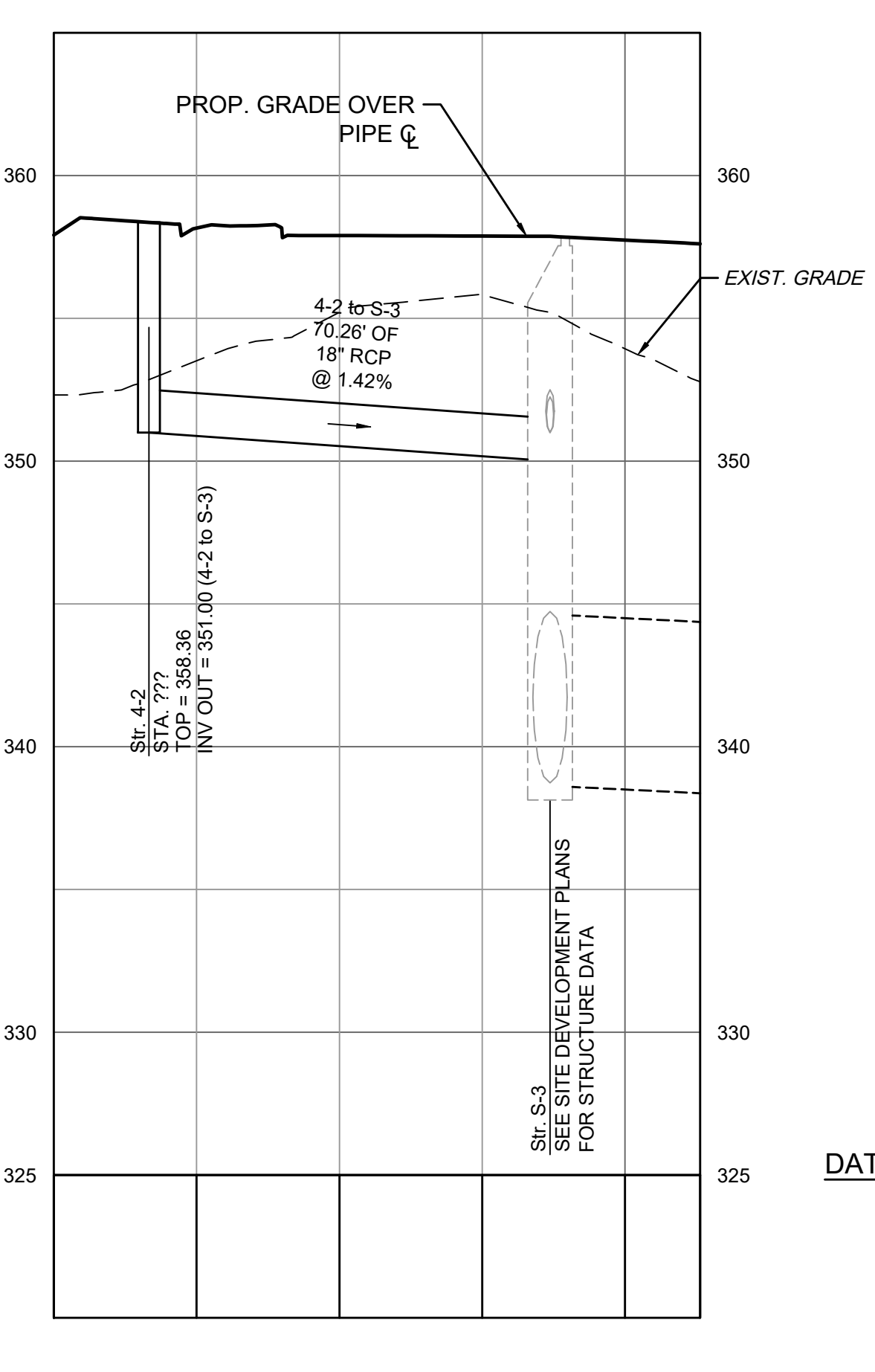
STORM SEWER PROFILES

JOB NO. 41073.009
SHEET NO. 06(2)

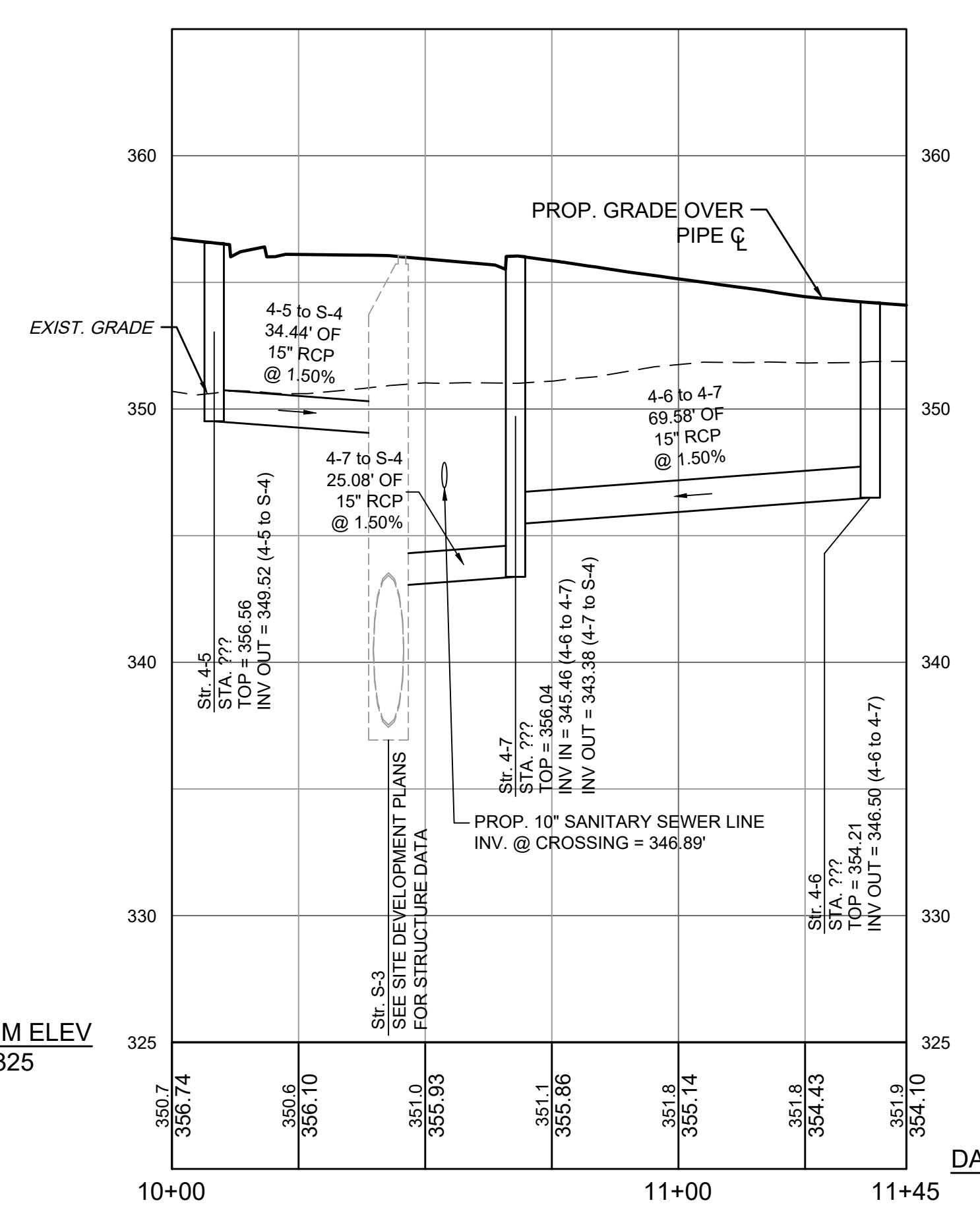
STR. 3-7 TO STR. 3-9
PROP. STORM SEWER PROFILE



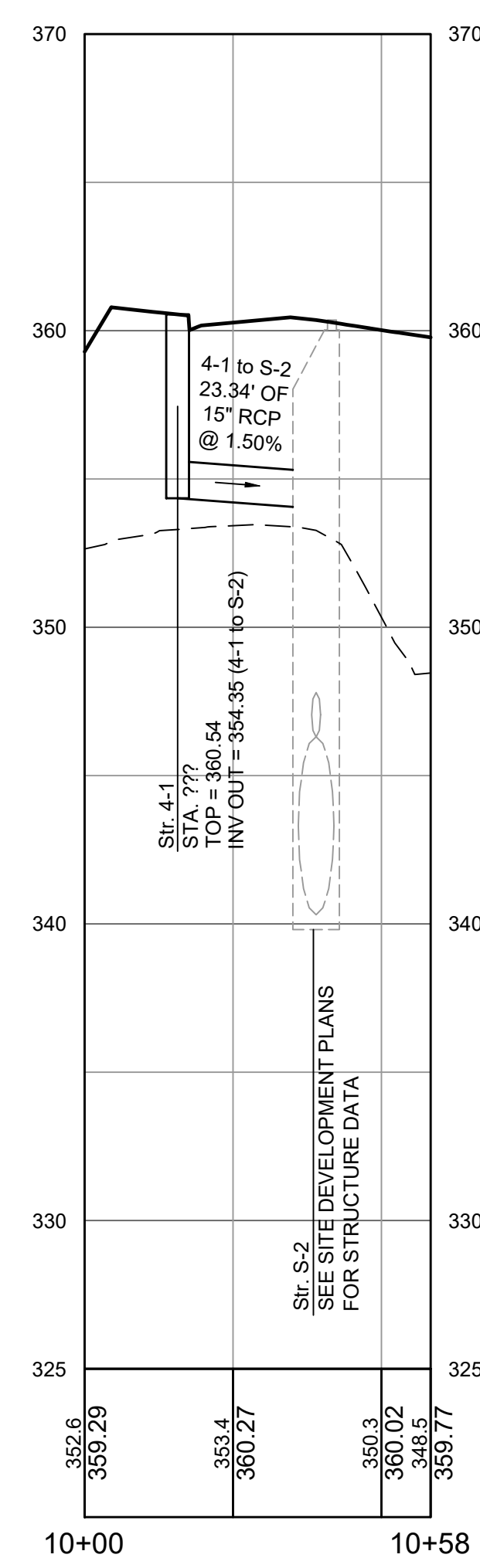
STR. 4-2 TO STR. S-3
PROP. STORM SEWER PROFILE



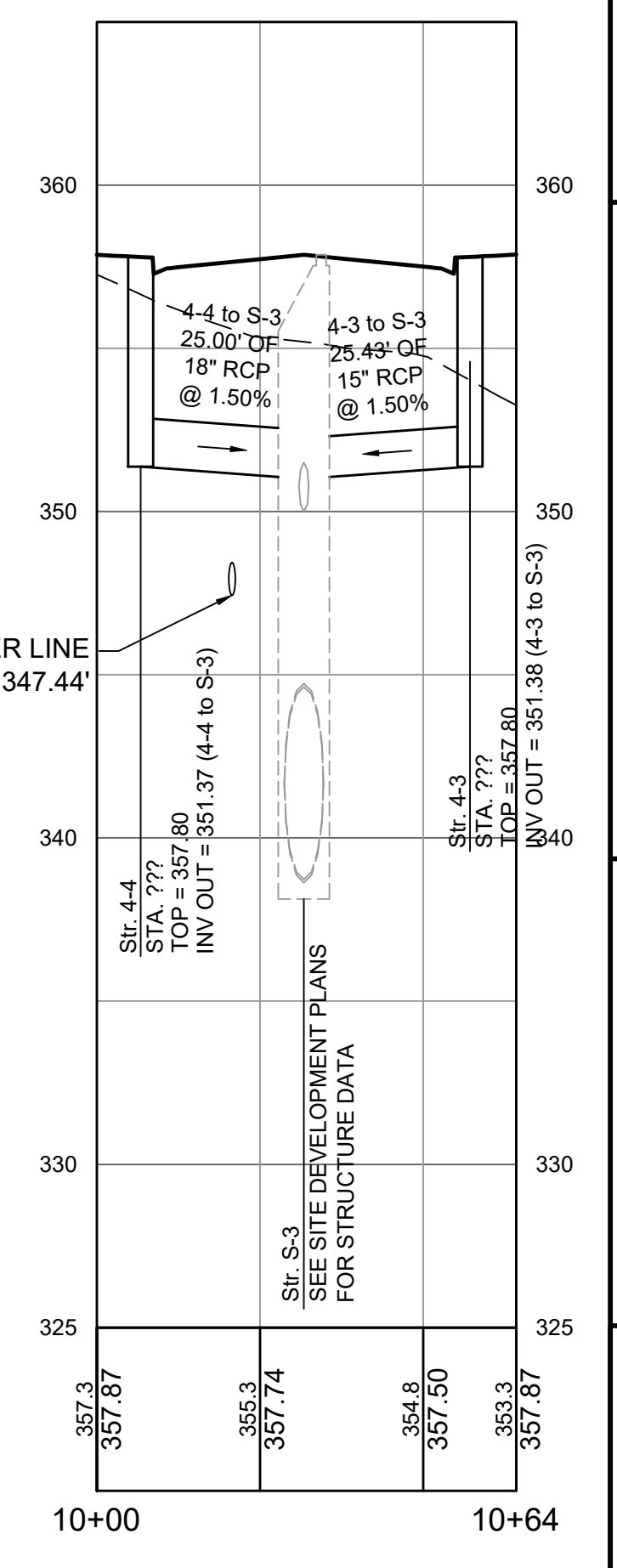
STR. 4-5 TO STR. S-4
PROP. STORM SEWER PROFILE



STR. 4-1 TO STR. S-2
PROP. STORM SEWER PROFILE



STR. 4-3 TO STR. S-3
PROP. STORM SEWER PROFILE



PLAN VIEW

ELEVATION VIEW

UNIT DESIGNATION	INSIDE VAULT DIMENSIONS (L x W)	FILTERRA TREATMENT AREA
FTSC 4' x 4'	8' x 4'	4' x 4'
FTSC 6' x 4'	8' x 6'	6' x 4'
FTSC 6' x 6'	10' x 6'	6' x 6'
FTSC 8' x 6'	12' x 6'	8' x 6'
FTSC 10' x 6'	16' x 8'	10' x 6'
FTSC 12' x 8'	18' x 8'	12' x 8'

CONTECH ENGINEERED SOLUTIONS LLC
www.contechES.com
8025 Centre Pointe Drive, Suite 400, West Chester, OH 45391
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FILTERRA® WITH SEDIMENTATION CHAMBER

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PLAN VIEW STANDARD OUTLET RISER

SECTION A-A

STORMFILTER DESIGN NOTES

STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. THE STANDARD MANHOLE STYLE IS SHOWN WITH THE MAXIMUM NUMBER OF CARTRIDGES (3). VOLUME SYSTEM IS ALSO AVAILABLE WITH MAXIMUM 3 CARTRIDGES. (Ø4 1219 mm) MANHOLE STORMFILTER PEAK HYDRAULIC CAPACITY IS 1.0 CFS (Ø3.3 L/s). IF THE SITE CONDITIONS EXCEED 1.0 CFS (Ø3.3 L/s) AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

CARTRIDGE SELECTION	27" (686 mm)	18" (458 mm)	LOW DROP
CARTRIDGE HEIGHT	3.60" (90 mm)	2.3" (58 mm)	1.8" (46 mm)
RECOMMENDED HYDRAULIC DROP (ft)	2.1 (30)	1.67 (1.08)	1 (0.65)
SPECIFIC FLOW RATE (gpm/ft²) (L/s/m²)	18.79 (1.19)	11.25 (0.71)	15 (0.95)
CARTRIDGE FLOW RATE (gpm) (L/s)	22.5 (1.42)	18.79 (1.19)	15 (0.95)

* 1.67 gpm/ft² (1.08 L/s/m²) SPECIFIC FLOW RATE IS APPROVED WITH PHOSPHOSORB® (PSORB) MEDIA ONLY

SITE SPECIFIC DATA REQUIREMENTS

STRUCTURE ID	*
WATER QUALITY FLOW RATE (cfs) (L/s)	*
PEAK FLOW RATE (cfs) (L/s)	*
RETURN PERIOD OF PEAK FLOW (yrs)	*
CARTRIDGE HEIGHT (SEE TABLE ABOVE)	*
NUMBER OF CARTRIDGES REQUIRED	*
CARTRIDGE FLOW RATE	*
MEDIA TYPE (PERLITE, ZPG, PSORB)	*

GENERAL NOTES

- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- DIMENSIONS MARKED WITH () ARE REFERENCE DIMENSIONS. ACTUAL DIMENSIONS MAY VARY.
- FOR SITE SPECIFIC DRAWINGS WITH DETAILED VAULT DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE. www.contechES.com
- STORMFILTER WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- STRUCTURE SHALL MEET AASHTO HS-20 LOAD RATING. ASSUMING EARTH COVER OF 0' - 6" (1524 mm) AND GROUNDWATER ELEVATION AT, OR BELOW, THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION. CASTINGS SHALL MEET AASHTO M308 AND BE CAST WITH THE CONTECH LOGO.
- FILTER CARTRIDGES SHALL BE MEDIA-FILLED, PASSIVE, SIPHON ACTUATED, RADIAL FLOW, AND SELF-CLEANING. RADIAL MEDIA DEPTH SHALL BE 7 INCHES (178 mm). FILTER MEDIA CONTACT TIME SHALL BE AT LEAST 38 SECONDS.
- SPECIFIC FLOW RATE IS EQUAL TO THE FILTER TREATMENT CAPACITY (gpm) (L/s) DIVIDED BY THE FILTER CONTACT SURFACE AREA (sq ft) (sq m).
- STORMFILTER STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO ASTM C-478 AND AASHTO LOAD FACTOR DESIGN METHOD.

INSTALLATION NOTES

- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STORMFILTER STRUCTURE.
- CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
- CONTRACTOR TO PROVIDE, INSTALL AND GROUT INLET PIPE(S).
- CONTRACTOR TO PROVIDE AND INSTALL CONNECTOR TO THE OUTLET RISER STUB. STORMFILTER EQUIPPED WITH A DUAL DRAIN HDPE OUTLET STUB AND SAND COLLAR. IF OUTLET PIPE IS LARGER THAN 8 INCHES (200 mm), CONTRACTOR TO REMOVE THE 8 INCH (200 mm) OUTLET STUB AT MOLD-BED-IN CUT LINE. COURTESY BY FERRO-COR EQUAL, AND PROVIDED BY CONTRACTOR.
- CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.

CONTECH ENGINEERED SOLUTIONS LLC
www.contechES.com
8025 Centre Pointe Dr., Suite 400, West Chester, OH 45399
800-338-1122 513-645-7000 513-645-7993 FAX

SFMH48 STORMFILTER STANDARD DETAIL

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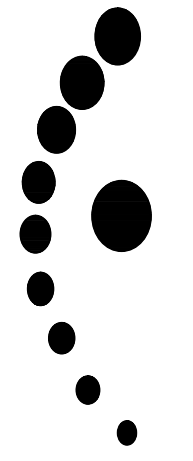
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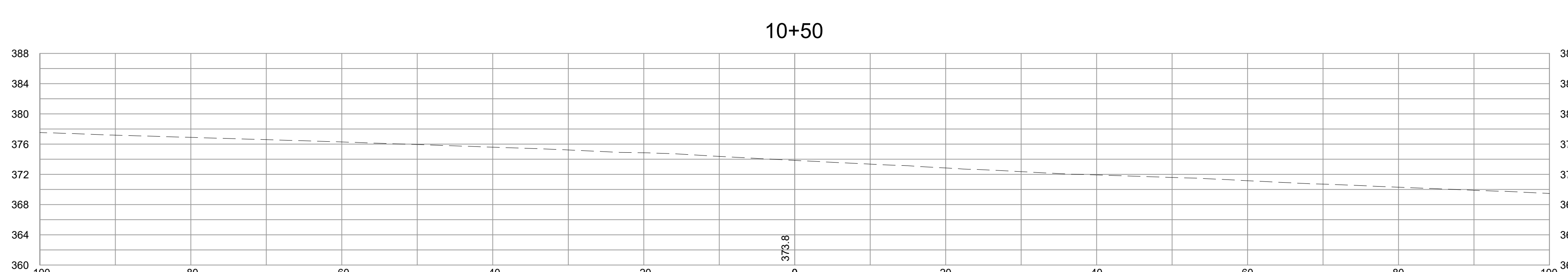
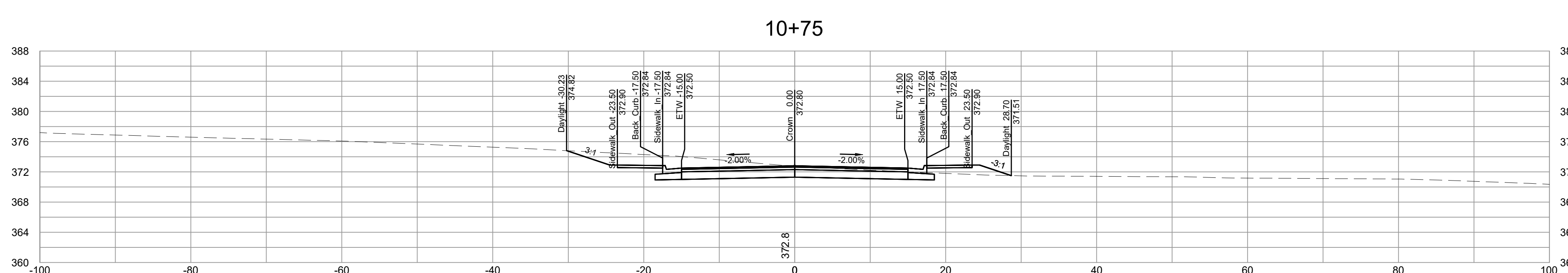
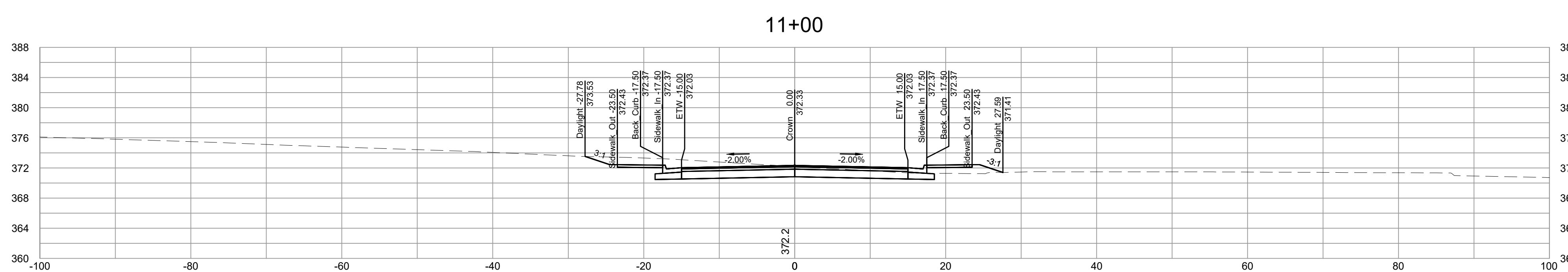
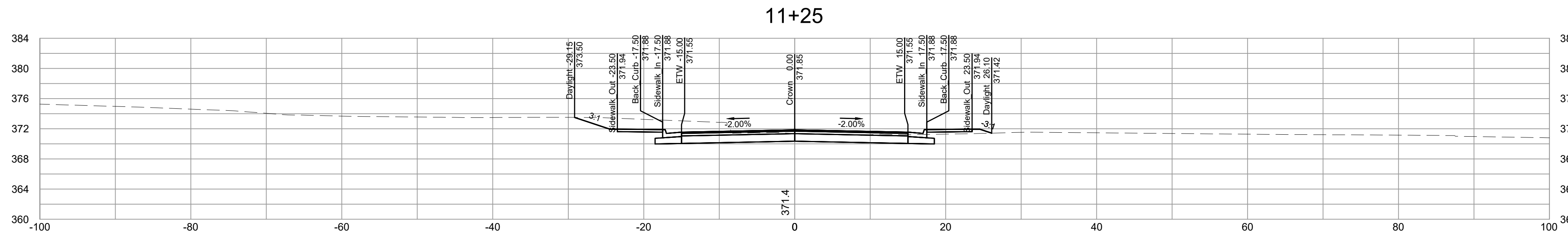
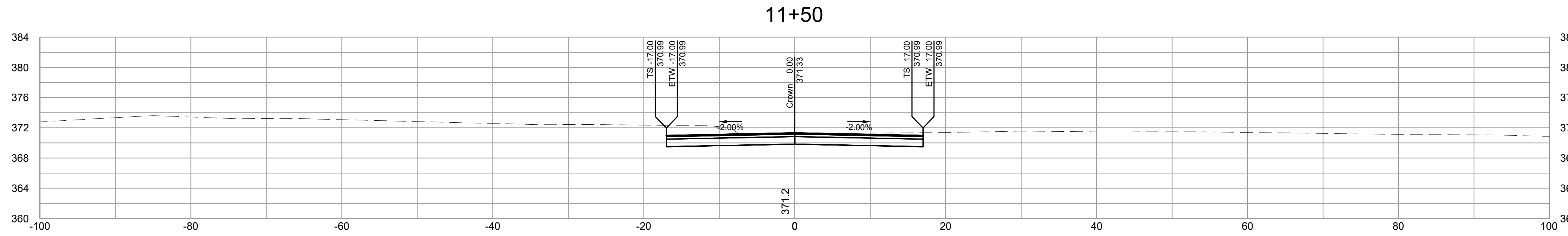
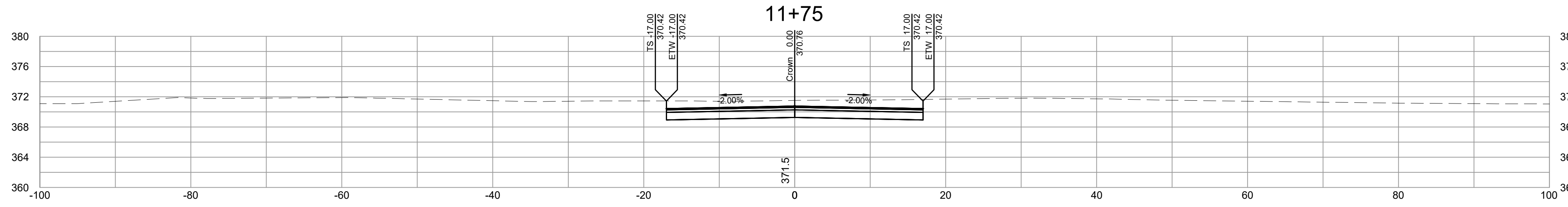
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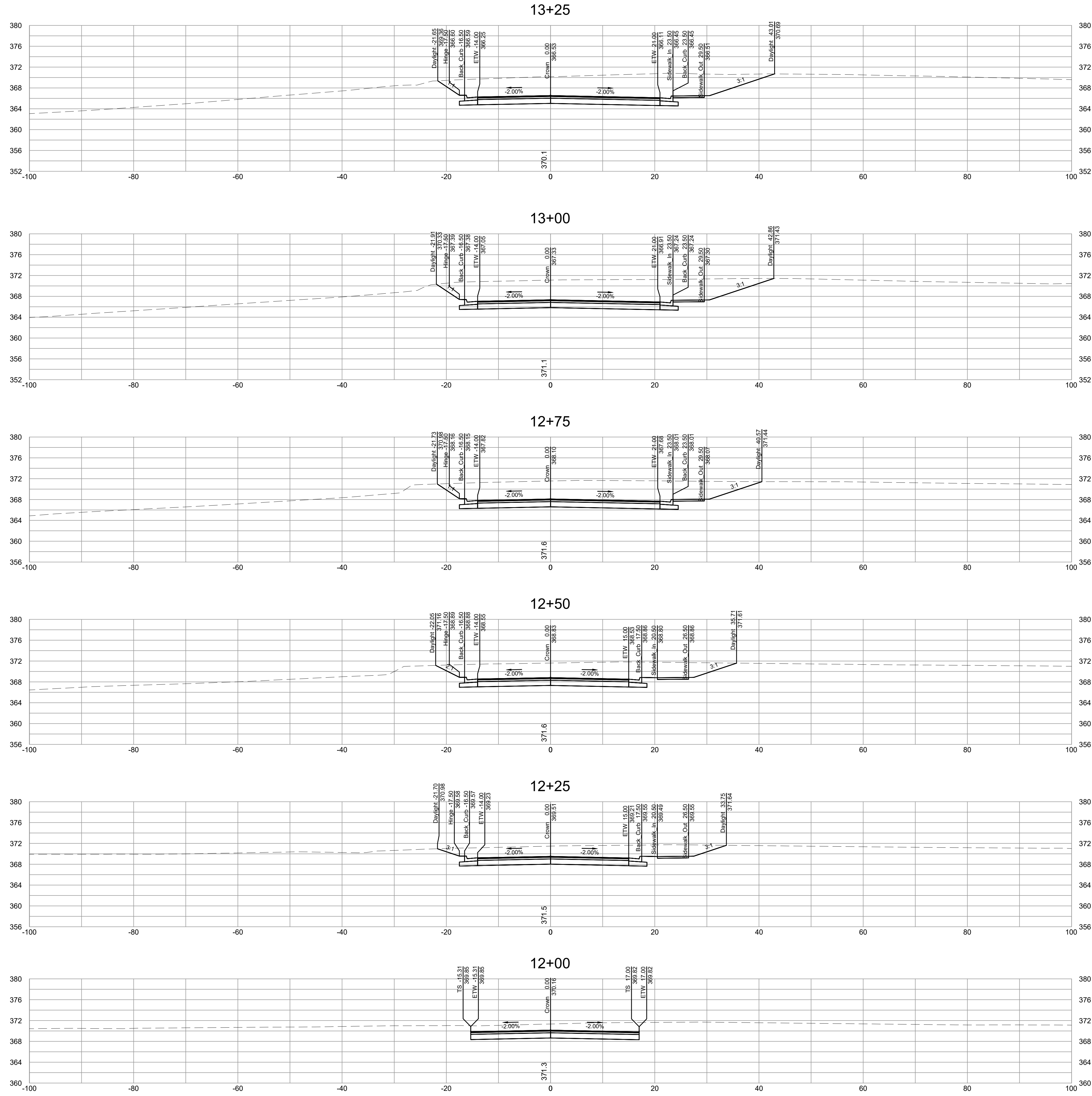
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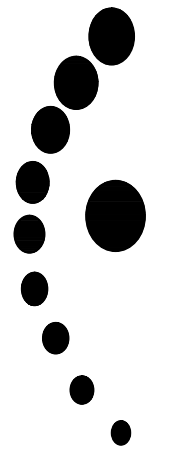
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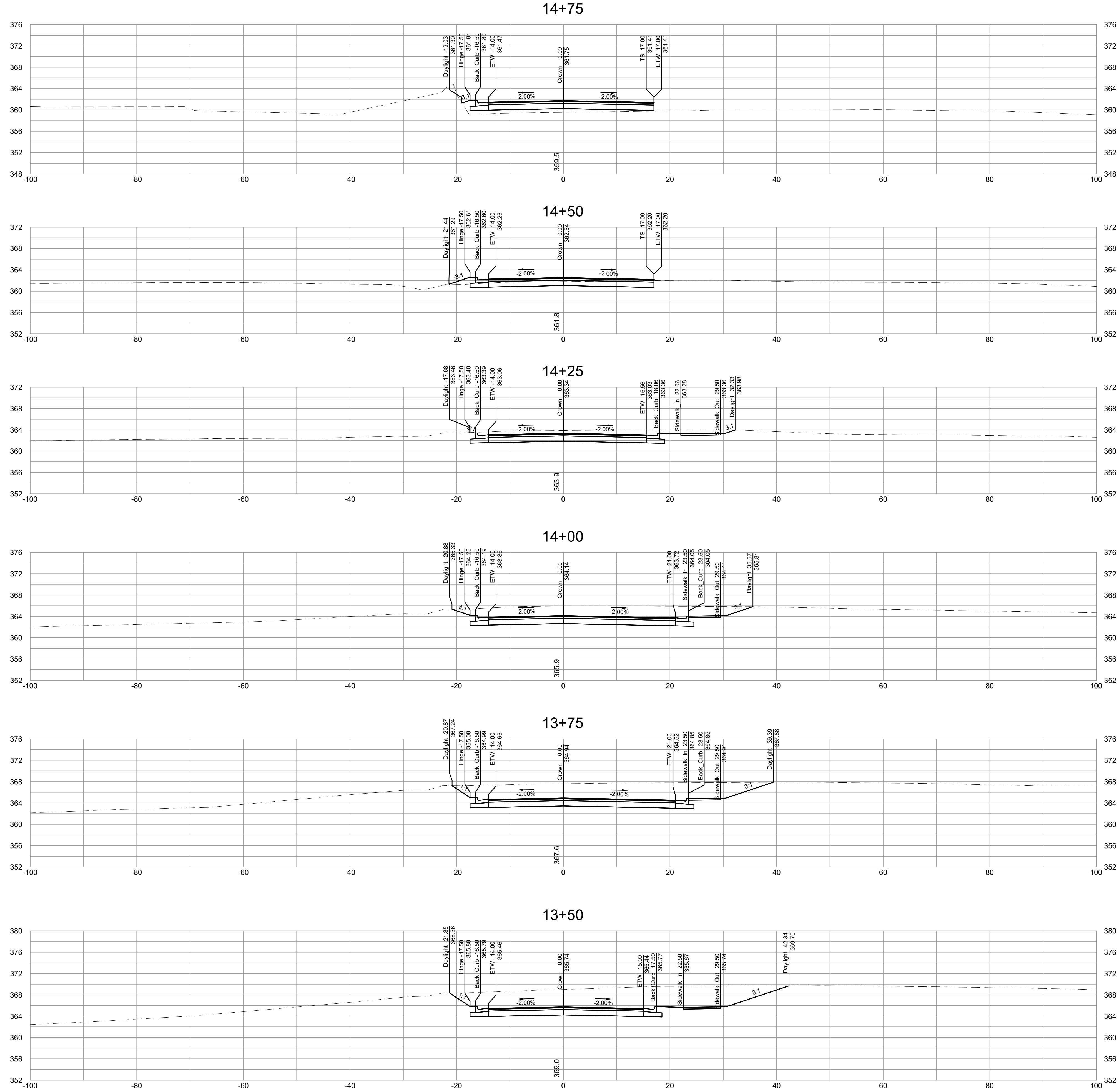
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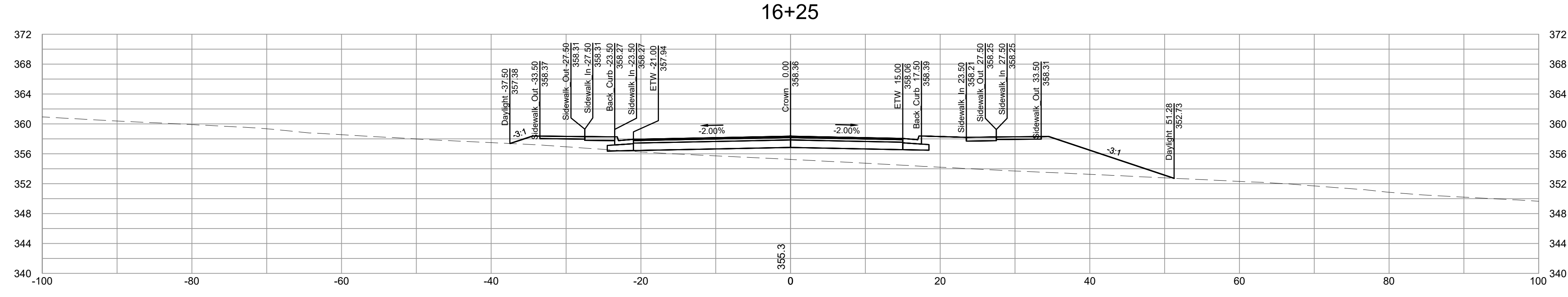
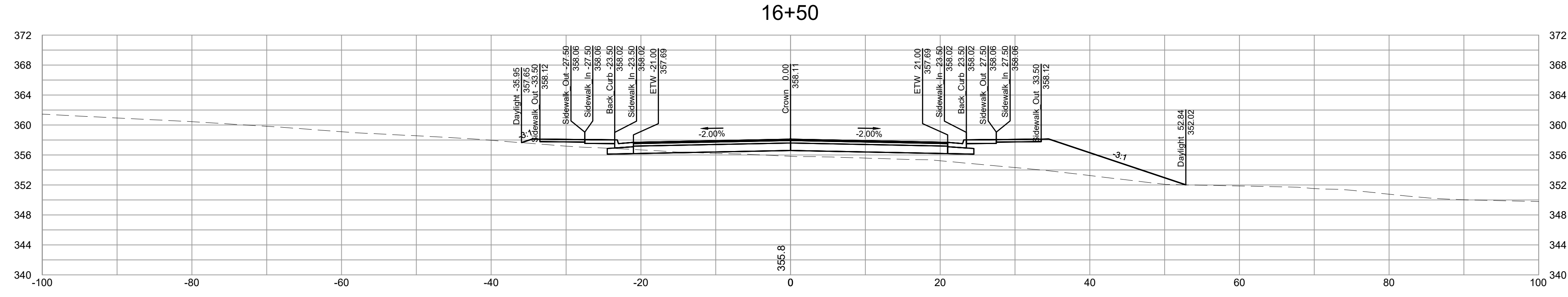
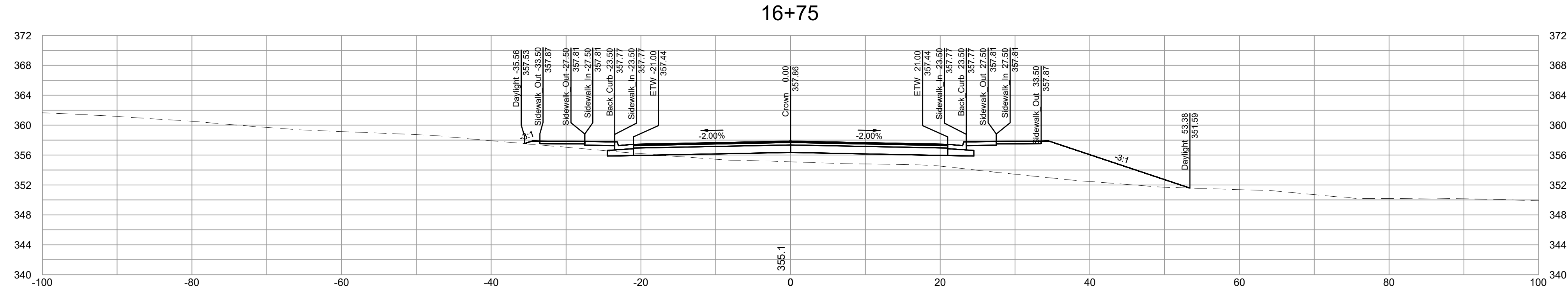
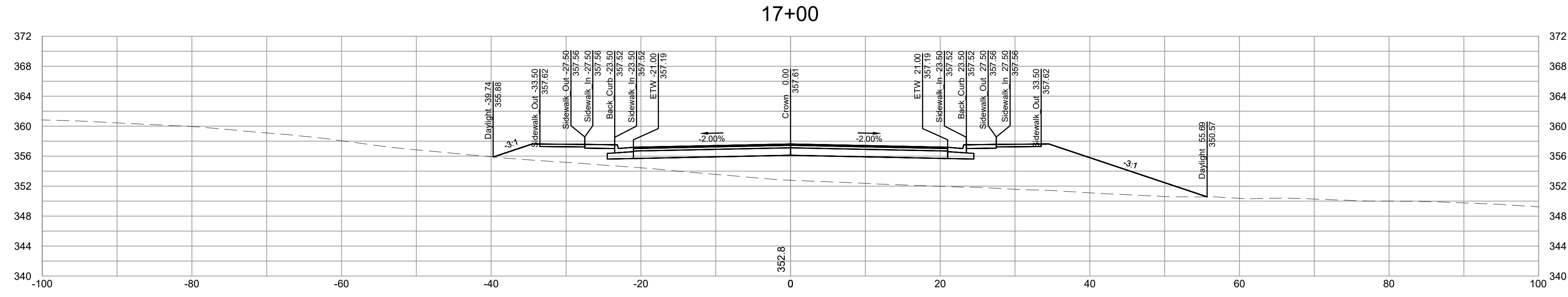
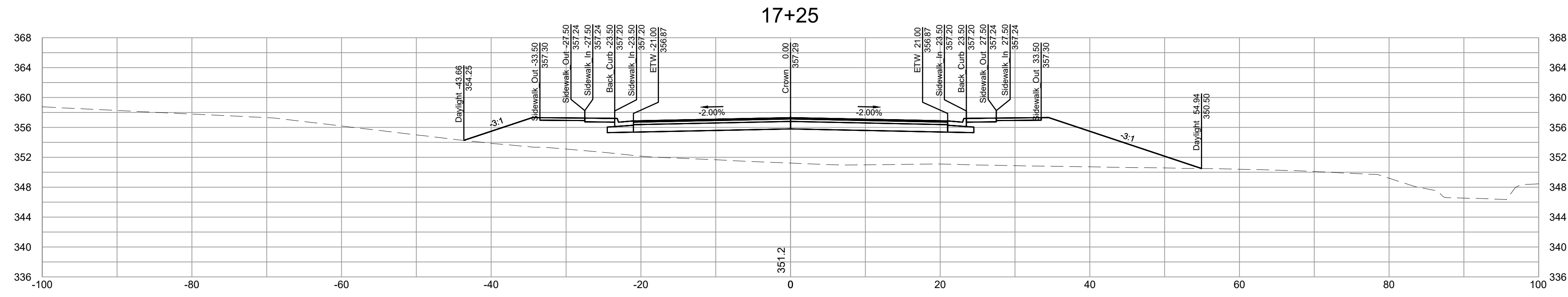
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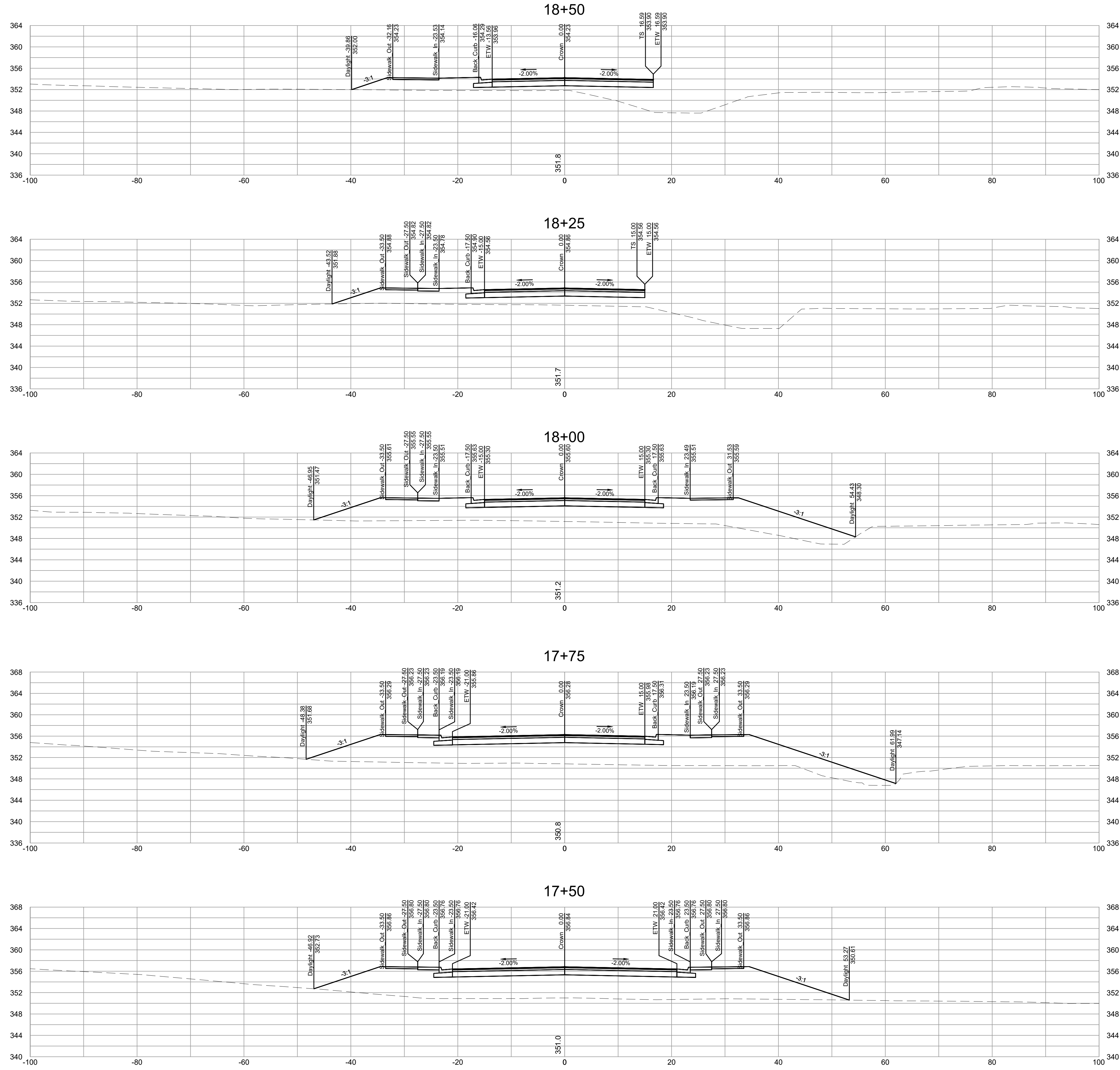
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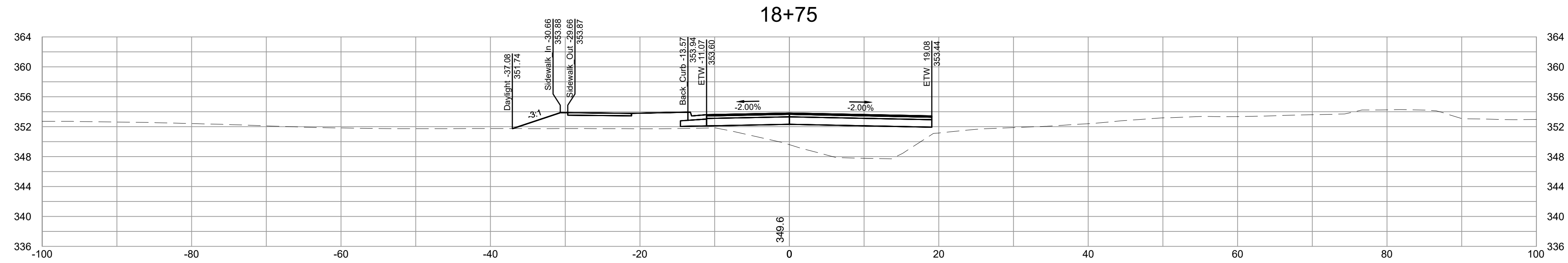
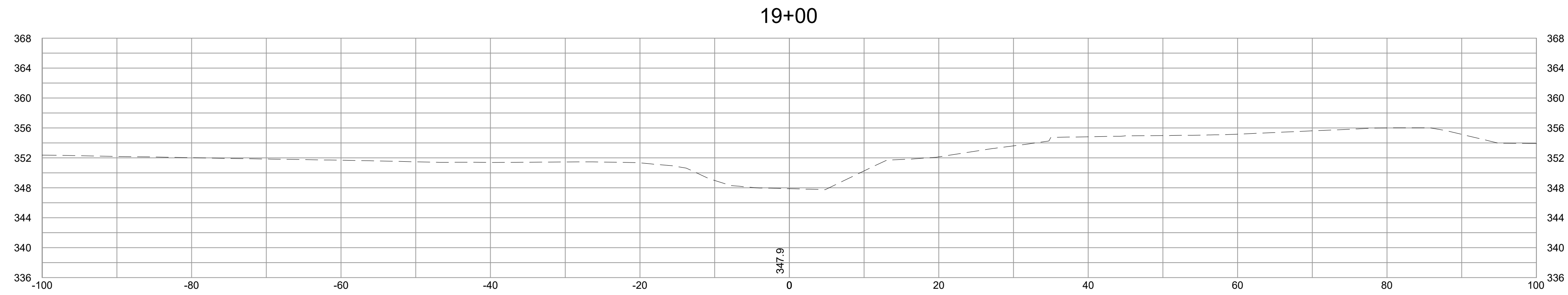
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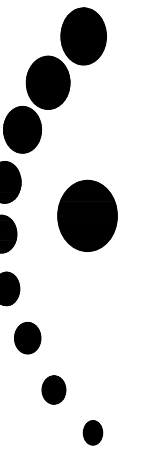
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